



RESIDENTIAL
PLANNED DEVELOPMENT
MASTER PLAN

Prepared for:

**Champion
Homes** 

OCTOBER 12, 1998

Prepared By:

The KTG Y Group
Architecture / Planning

STB
Landscape Architecture

VTN
Civil Engineering

IRON MOUNTAIN RANCH

LAS VEGAS, NEVADA

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CHAPTER 1

STATEMENT OF INTENT

1.1 IRON MOUNTAIN RANCH RESIDENTIAL PLANNED DEVELOPMENT (R-PD)

The Iron Mountain Ranch master plan shall reflect the objectives set forth in the Residential Planned Development (R-PD) District, which is a Special Purpose District as described in the City of Las Vegas Zoning Code. The proposed underlying General Plan designation for the property is Planned Community Development (PCD), allowing a mix of residential uses and densities based upon a master development plan.

The intent of the R-PD District is to permit and encourage comprehensively planned developments whose purpose is redevelopment, economic development, cultural enrichment, or to provide a single-purpose or multi-use planned developments. The Iron Mountain Ranch master plan accomplishes the following goals of the R-PD district:

- An orderly and creative arrangement of land uses that are harmonious and beneficial to the northwest Las Vegas area.
- A variety of housing types.
- Flexibility in the distribution of land uses, in the density of development, and in other matters typically regulated in zoning districts.
- Providing for recreational facilities in a planned setting.
- Development in accordance with a uniform set of standards and guidelines.
- Providing area-sensitive site planning and design.
- Contributing to the health, safety and general welfare of the community and providing development which is compatible with the City's goals and objectives.

1.2 RELATIONSHIP TO OTHER DOCUMENTS

The Iron Mountain Ranch Residential Planned Development Master Plan is the primary document for use by all entities undertaking any improvements, participating builders, and individual homeowners, including their respective sub-associations. The

R-PD Master Plan, including the provisions for a design review committee, ensure that the entire Iron Mountain Ranch project is developed as proposed.

All development plans shall comply with the adopted Iron Mountain Ranch R-PD Master Plan, CC&R's, and all other applicable regulations in the city, county, state, and federal jurisdictions.

In the event that one portion of the R-PD Master Plan is deemed unenforceable, all other portions of the R-PD Master Plan shall remain valid and enforceable.

1.3 ADJACENT PARCELS

In the event that adjacent parcels not currently within the master plan area are acquired, then such parcels will be subject to the same criteria contained herein.

CHAPTER 2

OBJECTIVES OF PROJECT

2.1 OVERALL PROJECT OBJECTIVE

Iron Mountain Ranch is a ±534 acre master planned community located within the Northwest General Plan Amendment area of Las Vegas. The master plan area is bisected by the Las Vegas grid system of arterial and collector roadways, as well as local streets providing access to privately owned parcels not a part of the master plan. In order to ensure that a strong overall community concept is achieved within this physical context, the proposed master plan addresses three levels of planning, including:

- *Level 1 - Master Planning*, focusing on the creation of an overall community identity through themed entries and signage, consistent wall treatments, regional trails, perimeter landscaping and streetscape treatments.
- *Level 2 - Site Planning*, focusing on establishing a strong integration among neighborhoods, as well as from each neighborhood to the overall master plan community.
- *Level 3 - Architecture*, setting minimum standards to ensure the creation of pleasantly diverse neighborhoods which are compatible with the overall master plan community.

2.2 MASTER PLAN CONCEPT

The master plan for Iron Mountain Ranch creates a series of villages whose boundaries are defined by the City of Las Vegas' existing or planned network of arterials, collectors, and local roadways within the Northwest General Plan Amendment area. Together the villages provide an array of housing choices for future residents with lots ranging in size from 4,500 square feet to 30,000 square feet.

The overall community identity is described as "*Desert Harmony*", reflecting the unique beauty of the project's desert setting. This identity is defined through the use of architectural styles, design elements and materials, which are responsive to and compatible with the natural desert character of the site. The overall development of Iron Mountain Ranch and its community elements are designed to reinforce this overall design concept. The community elements will include the following:

- Themed entries at major intersections using enhanced wall treatments, landscaping and signage to identify primary gateways into Iron Mountain Ranch.
- Themed entries into individual subdivisions with designs and materials which are compatible with the overall community.
- The use of a consistent landscape treatment along perimeter roadways to unify the overall master plan area.
- The use of consistent project walls at neighborhood edges adjacent to perimeter roadways.
- The use of City of Las Vegas master planned trails along Bradley Road and Grand Teton Drive to link the villages with on-site and off-site community amenities.

Please refer to Chapter 7 for more detailed discussion and exhibits depicting specific architectural styles and the various community elements which reinforce the Desert Harmony theme.

2.3 MASTER PLAN INTEGRATION INTO REGIONAL CONTEXT

Recognizing the existing rural character of the surrounding area, the development of Iron Mountain Ranch will ensure the continuity of this lifestyle through the provision of large equestrian lots up to 30,000 square feet each. In addition, the plan provides for the construction of City approved regional equestrian trails along Bradley Road and Grand Teton Drive within the master plan area. These trails will provide access to regional recreational facilities located in the Northwest General Plan Area.

Iron Mountain Ranch integrates into the fabric of the northwest area by providing transitional buffer lots of 30,000 square feet, 20,000 square feet and 10,000 square feet adjacent to neighboring Desert Rural properties as described below. All lots are net of public right-of-way.

2.3.1 30,000 S.F. Buffer Lots

The 30,000 square foot buffer lots have been established along portions of Jones Boulevard and Bradley Road in Planning Areas 1, 8, and 15. In order to integrate with neighboring areas, these lots will face the perimeter streets (Jones Boulevard and Bradley Road), rather than be oriented inward behind project walls. The lots along Jones Boulevard feature a private frontage drive to provide access to individual lots.

The lots facing Bradley Road have direct access to the street. All 30,000 square foot lots will allow equestrian uses on-site.

2.3.2 20,000 S.F. Buffer Lots

The 20,000 S.F. buffer lots are located in Planning Areas 1, 7, 8, 10, 12, 16, and 17. These lots are provided to blend into the rural lifestyle of adjacent properties. All 20,000 S.F. lots will allow equestrian uses on-site. The four lots along the north boundary of Planning Area 10 are limited to single story homes.

2.3.3 10,000 Square Foot Buffer Lots

The 10,000 square foot buffer lots are located in Planning Area 3 along Whispering Sands Drive. These lots will face the perimeter street (Whispering Sands Drive) and will have direct access. Equestrian uses will not be permitted on the 10,000 square foot lots.

2.4 STATISTICAL SUMMARY

The following Statistical Summary shows that a maximum of 2,039 dwelling units are allowed calculated according to the maximum allowable density for each land use designation within the 534 acre master plan area. The resulting overall density is 3.8 DU/Ac. The Statistical Summary also shows the approximate number of proposed units based upon the Conceptual Site Plan as $\pm 1,886$, a reduction of 153 units from the maximum allowable. As the site plans are refined during the tentative mapping process, the actual number of proposed units may increase by a maximum of 5% above the 1,886 as currently indicated on the Conceptual Site Plan. In no case shall the total number of units exceed 2,039 within the 534 acre master plan area.

Likewise, as project development progresses, the master developer shall have the ability to transfer units from one planning area to another planning area or from one village to another village, as long as the number of units transferred does not exceed 5% of the allowable number of units for that planning area or village. Under no circumstances does this allow the maximum dwelling units per acre as listed in the Statistical Summary to be exceeded for any individual planning area.

Planning areas which are not currently within the master plan area will require annexation into the R-PD Master Plan, potentially increasing the overall unit count of the master plan area. For informational purposes only, the statistical summary shows the increased unit count which would be allowed subject to the annexation of Planning Areas 23 and 25.

PLAN AREA	LAND USE / ZONING DESIGNATIONS	MIN. LOT SIZE (Net of Public R.O.W.)	GROSS ACRES	MAX. DU/AC	MAX. UNITS	APPROX. PROPOSED UNITS
VILLAGE 1						
3	R / RPD-3	7,000 S.F. 10,000 S.F.	35.0	3.49	122	112 10
2	R / RPD-3	7,000 S.F.	28.0	3.49	97	97
1	DR / RPD-2	30,000 S.F. 20,000 S.F.	8.0	2.0	16	9 6
			71.0		235	234
VILLAGE 2						
5	L / RPD-5	4,500 S.F.	40.0	5.49	219	195
			40.0		219	195
VILLAGE 3						
8	DR / RPD-2	20,000 S.F. 30,000 S.F.	39.5	2.0	79	50 10
12	DR / RPD-2	20,000 S.F.	29.5	2.0	59	45
7	DR / RPD-2	20,000 S.F.	31.0	2.0	62	45
			100.0		200	150
VILLAGE 4						
13	L / RPD-5	4,500 S.F.	40.0	5.49	219	195
11	L / RPD-5	4,500 S.F.	30.0	5.49	164	158
10,16	DR / RPD-2	20,000 S.F.	30.0	2.0	60	48
			100.0		443	401
VILLAGE 5						
14	R / RPD-3	10,000 S.F.	40.0	2.5	100	100
19	R / RPD-3	7,000 S.F.	40.0	3.49	139	139
			80.0		239	239

VILLAGE 6

21	L / RPD-5	4,500 S.F.	40.0	5.49	219	215
18	L / RPD-5	4,500 S.F.	40.0	5.49	219	200
20	L / RPD-5	4,500 S.F.	40.0	5.49	219	219
15	DR / RPD-2	30,000 S.F.	9.0	2.0	18	9
17	DR / RPD-2	20,000 S.F.	14.0	2.0	28	24
			143.0		703	667
TOTAL			534.0	3.8	2,039	1,886

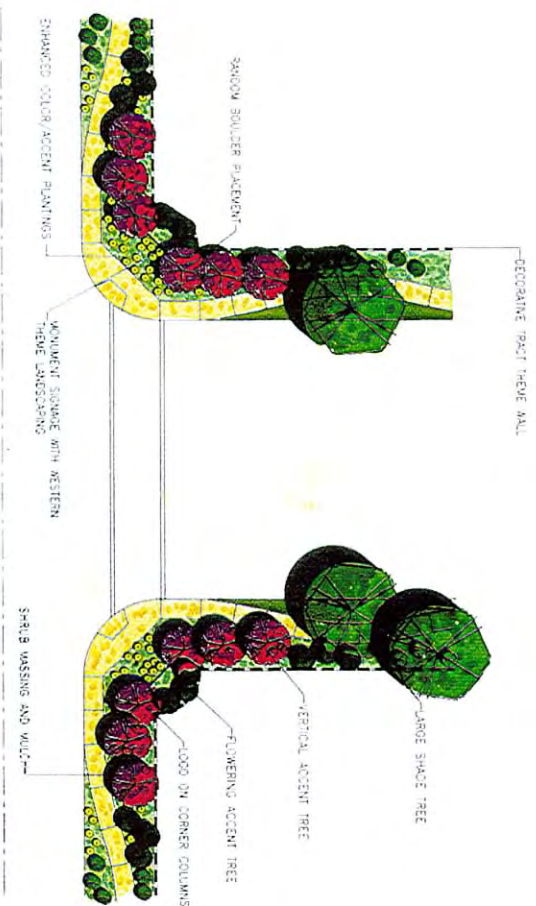
VILLAGE 6 (Annexation Area - For Informational Purposes Only)

23	R / RPD-3	7,000 S.F. 20,000 S.F.	40.0	3.49	139	124 8
25	R / RPD-3	10,000 S.F. 20,000 S.F.	40.0	2.5	100	92 8
			160.0		496	
GRAND TOTAL			614.0	3.7	2,278	2,118

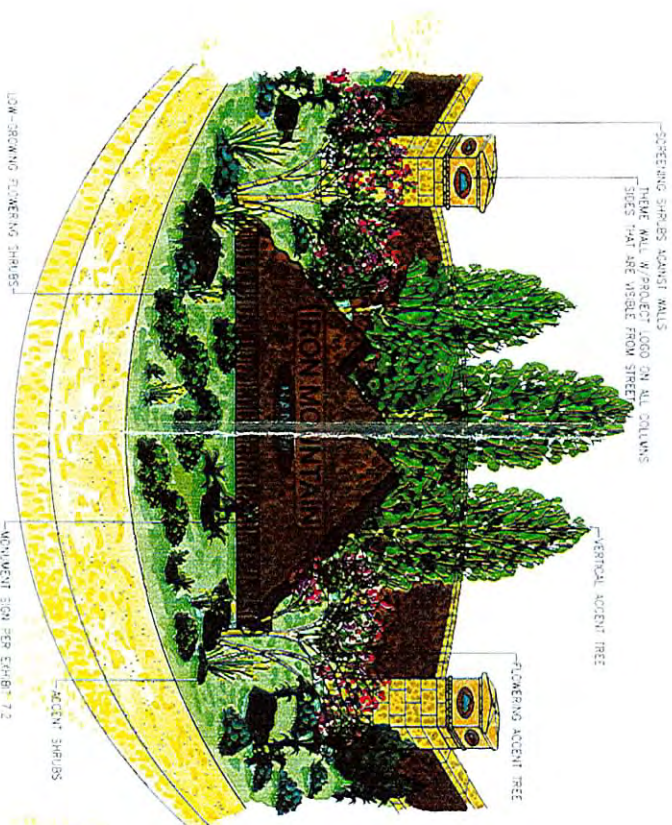
OPEN SPACE SUMMARY

OPEN SPACE REQUIRED	
Density X .0165 X Gross Acres	30.8
(3.5 x .0165 X 534 = 30.8)	
<hr/>	
OPEN SPACE PROVIDED	30.8
<hr/>	
CITY PARK*	11.0
NEIGHBORHOOD PARKS	10.0
30' MULTI-PURPOSE TRAIL (24' Open Space Credit)	0.7
20' MULTI-PURPOSE TRAIL (14' Open Space Credit)	2.7
25' EQUESTRIAN TRAIL (19' Open Space Credit)	2.4
GREENBELTS	1.0
PARKWAYS (Thematic Loop St's & Trail St's)	3.0
TOTAL	30.8

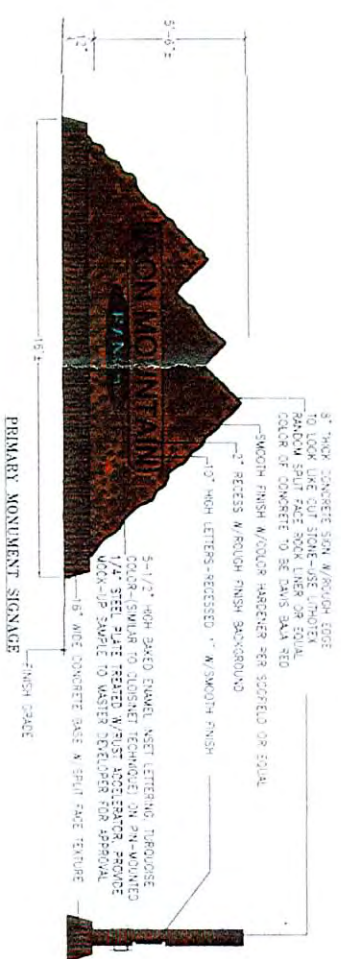
* City park acreage is in addition to 534 Acre master plan area. For each acre of required open space that the master plan is deficient, one acre of the city park will be developed to city standards by the Master Developer. The ultimate amount of city park acreage developed by the Master Developer is dependent upon the final density approved for the R-PD Master Plan. Changes to the density will result in adjustments to the city park acreage as well.



A PRIMARY ENTRY-PLAN VIEW
NOT TO SCALE



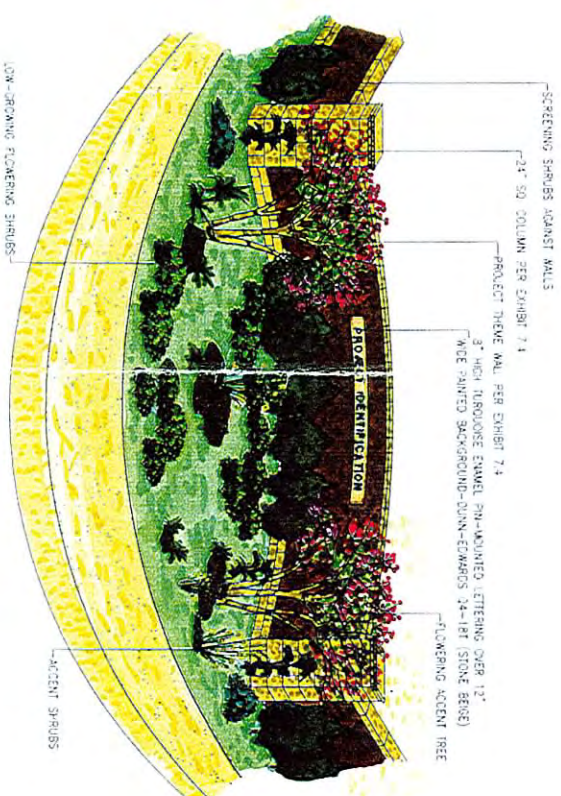
B PRIMARY ENTRY-ELEVATION
NOT TO SCALE



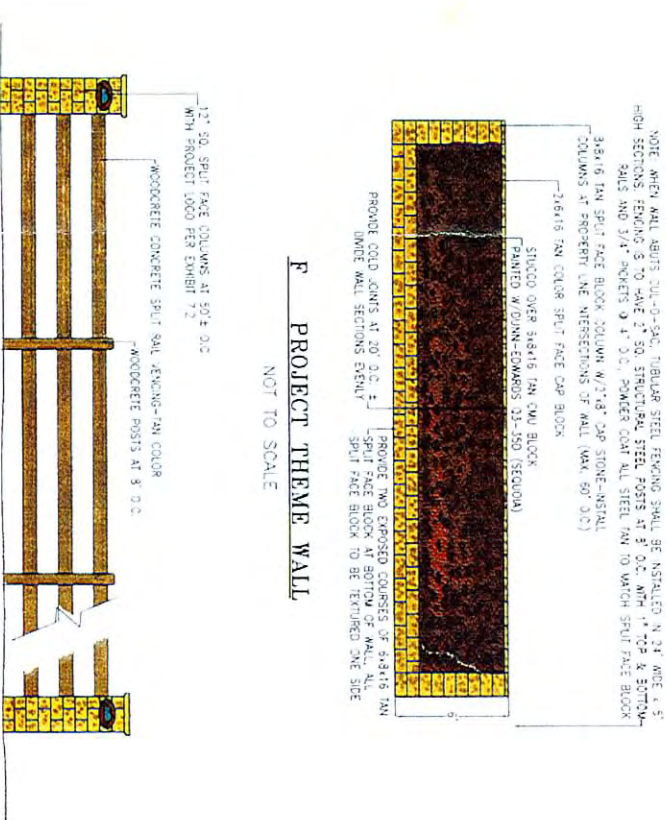
E MONUMENT SIGN WITH ENTRY WALL
NOT TO SCALE



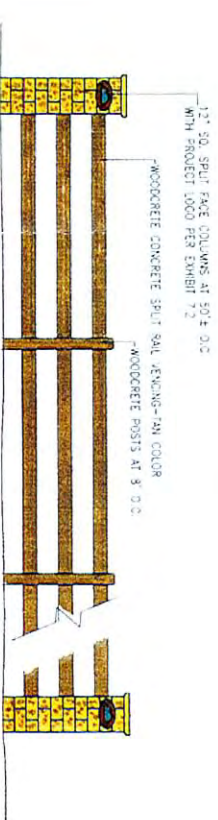
C PEDESTRIAN GREENBELT
NOT TO SCALE



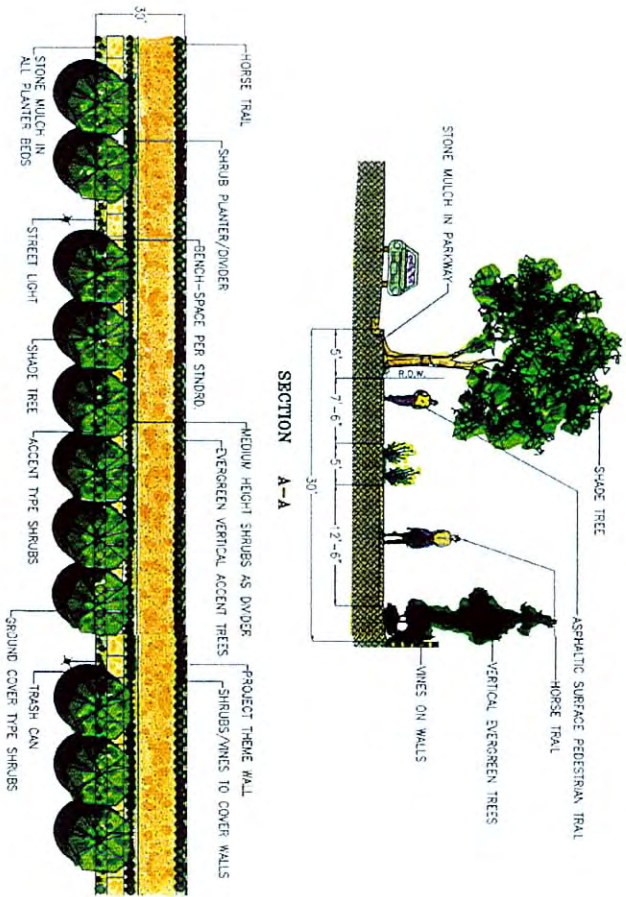
D NEIGHBORHOOD ENTRY-ELEVATION
NOT TO SCALE



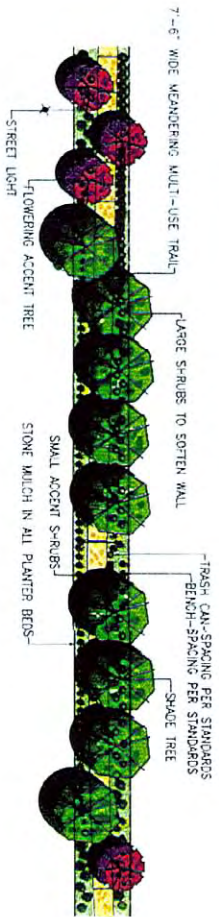
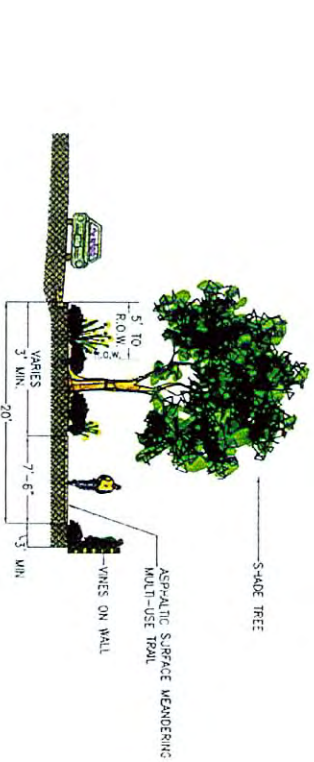
F PROJECT THEME WALL
NOT TO SCALE



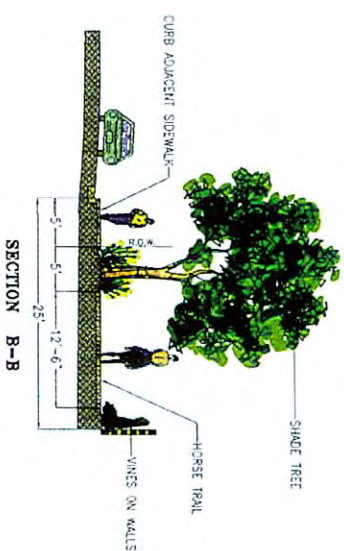
G SPLIT RAIL FENCE AT PRIVATE DRIVES
NOT TO SCALE



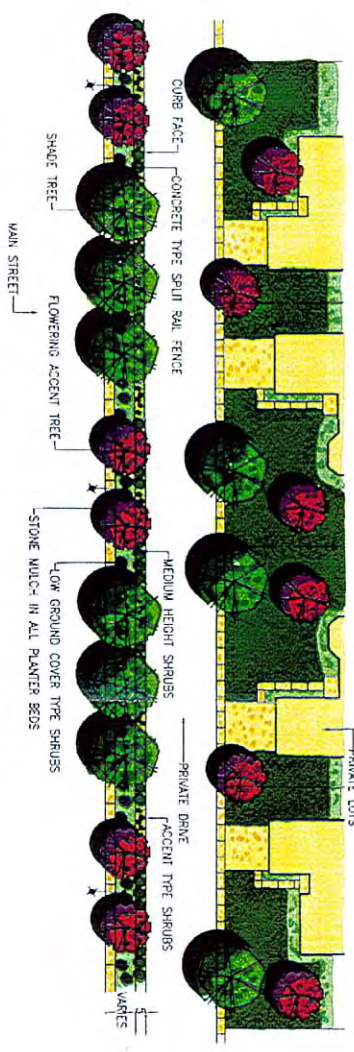
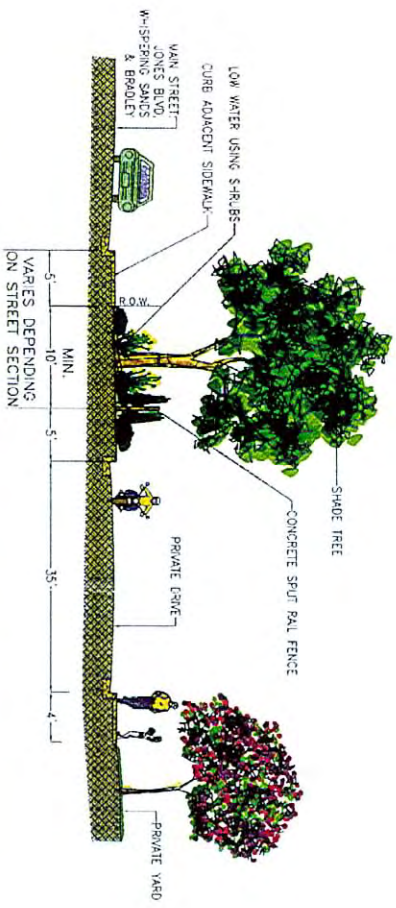
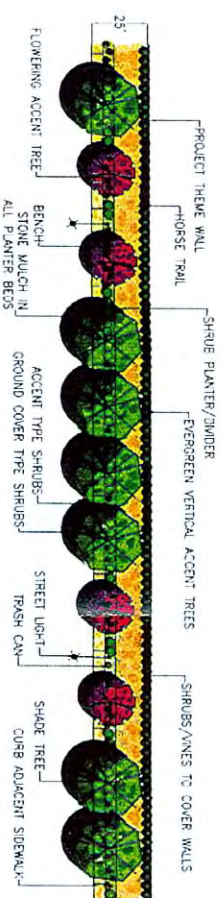
G MULTI-PURPOSE TRAIL
NOT TO SCALE



I MULTI-PURPOSE TRAIL (EQUESTRIAN EXCLUDED)
NOT TO SCALE



H HORSE TRAIL
NOT TO SCALE



J PRIVATE DRIVE LANDSCAPE TREATMENT
NOT TO SCALE

LANDSCAPE ELEMENTS SECTIONS AND ELEVATIONS

IRON MOUNTAIN RANCH

LAS VEGAS * NEVADA

CHAPTER 3 LOCATION

3.1 SITE DESCRIPTION

Iron Mountain Ranch, consisting of numerous parcels totalling approximately 534 acres, is located in the Northwest General Plan Amendment area of Las Vegas. It is generally bounded by Jones Boulevard on the west, Decatur Boulevard on the east, Iron Mountain Road on the north, and Whispering Sands Drive on the south. The site is characteristic of typical desert terrain, with generally consistent slopes in the range of ½ % to 1 %.

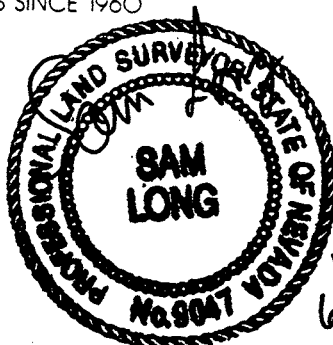
Nearby amenities include large amounts of park, recreation and open space lands, as identified in the Northwest General Plan Amendment. These areas are generally located to the north and west of Iron Mountain Ranch and include the Qual Springs Wilderness Study Area.

V1116

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JUNE 11, 1998
BY: SL
P.R. BY: MSC
PAGE 1 OF 4

EXPLANATION:

THIS LEGAL DESCRIBES A PARCEL OF LAND GENERALLY LOCATED BETWEEN JONES BOULEVARD AND DECATUR BOULEVARD AND BETWEEN IRON MOUNTAIN ROAD AND WHISPERING SANDS DRIVE FOR ZONING PURPOSES.

**IRON MOUNTAIN RANCH
LEGAL DESCRIPTION FOR ZONING**

PARCEL I:

THAT PORTION OF THE SOUTHWEST QUARTER (SW 1/4) OF SECTION 12 AND THAT PORTION OF THE NORTH HALF (N 1/2) OF THE NORTHWEST QUARTER (NW 1/4) OF SECTION 13, TOWNSHIP 19 SOUTH, RANGE 60 EAST, M.D.M., CLARK COUNTY, NEVADA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHWEST CORNER OF THE SOUTHWEST QUARTER (SW 1/4) OF SAID SECTION 12 BEING ALSO THE NORTHWEST CORNER OF THE NORTH HALF (NW 1/2) OF THE NORTHWEST QUARTER (NW 1/4) OF SAID SECTION 13, SAME BEING THE CENTERLINE INTERSECTION OF JONES BLVD., (100 FOOT WIDE ROW) AND "GRAND TETON DRIVE, (100 FOOT WIDE ROW); THENCE SOUTH 00°38'35" WEST, ALONG THE WEST LINE OF SAID NORTH HALF (N 1/2), COINCIDENT WITH THE CENTERLINE OF SAID JONES BLVD., 1311.08 FEET TO THE SOUTHWEST CORNER OF SAID NORTH HALF (N 1/2), SAME BEING THE CENTERLINE INTERSECTION WITH WHISPERING SANDS DRIVE, (PROPOSED 60 FOOT WIDE ROW); THENCE NORTH 84°44'58" EAST, DEPARTING SAID WEST LINE AND SAID CENTERLINE, AND ALONG THE SOUTH LINE OF SAID NORTH HALF (N 1/2), COINCIDENT WITH THE CENTERLINE OF SAID WHISPERING SANDS DRIVE, 1251.75 FEET; THENCE NORTH 84°45'59" EAST, CONTINUING ALONG SAID SOUTH LINE AND SAID CENTERLINE, 1251.52 FEET TO THE SOUTHEAST CORNER OF SAID NORTH HALF (N 1/2), SAME BEING THE CENTERLINE INTERSECTION OF BRADLEY ROAD, (PROPOSED 80' WIDE ROW); THENCE NORTH 00°24'29" EAST, ALONG THE EAST LINE OF SAID NORTH HALF (N 1/2), COINCIDENT WITH THE CENTERLINE OF SAID BRADLEY ROAD, 1320.41 FEET TO THE NORTHEAST CORNER OF SAID NORTH HALF (N 1/2) OF THE NORTHWEST QUARTER (NW 1/4) OF SAID SECTION 13, ALSO BEING THE SOUTHEAST CORNER OF THE SOUTHWEST QUARTER (SW 1/4) OF SAID SECTION 12, SAME BEING THE CENTERLINE INTERSECTION WITH SAID GRAND TETON DRIVE; THENCE NORTH 01°12'34" WEST, CONTINUING ALONG THE CENTERLINE OF SAID BRADLEY ROAD, SAME BEING THE EAST LINE OF SAID SOUTHWEST QUARTER (SW 1/4), A DISTANCE OF 1361.86 FEET TO THE NORTHEAST CORNER OF THE SOUTHEAST QUARTER (SE 1/4) OF SAID SOUTHWEST QUARTER (SW 1/4) OF SAID SECTION 12; THENCE SOUTH 85°21'14" WEST, ALONG THE NORTH LINE OF THE SOUTHEAST QUARTER (SE 1/4) OF SAID SOUTHWEST QUARTER (SW 1/4), A DISTANCE OF 1261.95 FEET TO THE SOUTHEAST CORNER OF THE NORTHWEST QUARTER (NW 1/4) OF THE SOUTHWEST QUARTER (SW 1/4) OF SAID SECTION 12; THENCE NORTH 01°46'53" WEST, ALONG THE EAST LINE OF THE NORTHWEST QUARTER (NW 1/4) OF SAID SOUTHWEST QUARTER (SW 1/4), A DISTANCE OF 1379.60 FEET TO THE NORTHEAST CORNER OF SAID NORTHWEST QUARTER (NW

LEGAL DESCRIPTION

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PAGE 2 OF 4

1/4); THENCE SOUTH 86°10'30" WEST, ALONG THE NORTH LINE OF SAID NORTHWEST QUARTER (NW 1/4), A DISTANCE OF 637.47 FEET TO THE NORTHWEST CORNER OF THE NORTHEAST QUARTER (NE 1/4) OF THE NORTHWEST QUARTER (NW 1/4) OF THE SOUTHWEST QUARTER (SW 1/4) OF SAID SECTION 12; THENCE SOUTH 02°03'44" EAST, ALONG THE WEST LINE OF SAID NORTHEAST QUARTER (NE 1/4) OF THE NORTHWEST QUARTER (NW 1/4) OF THE SOUTHWEST QUARTER (SW 1/4) OF SAID SECTION 12, A DISTANCE OF 694.09 FEET TO THE NORTHEAST CORNER OF THE SOUTHWEST QUARTER (SW 1/4) OF THE NORTHWEST QUARTER (NW 1/4) OF THE SOUTHWEST QUARTER (SW 1/4) OF SAID SECTION 12; THENCE SOUTH 85°46'40" WEST, ALONG THE NORTH LINE OF SAID SOUTH SOUTHWEST QUARTER (SW 1/4), A DISTANCE OF 634.23 FEET TO THE NORTHWEST CORNER OF THE SOUTHWEST QUARTER (SW 1/4) OF THE NORTHWEST QUARTER (NW 1/4) OF SAID SOUTHWEST QUARTER (SW 1/4), SAME BEING IN THE CENTERLINE OF SAID JONES BLVD.; THENCE SOUTH 02°20'22" EAST, ALONG THE WEST LINE OF SAID SOUTHWEST QUARTER (SW 1/4) OF THE NORTHWEST QUARTER (NW 1/4) OF THE SOUTHWEST QUARTER (SW 1/4) OF SAID SECTION 12, COINCIDENT WITH SAID CENTERLINE OF JONES BLVD., A DISTANCE OF 698.39 FEET TO THE NORTHWEST CORNER OF THE NORTHWEST QUARTER (NW 1/4) OF THE SOUTHWEST QUARTER (SW 1/4) OF SAID SOUTHWEST QUARTER (SW 1/4) OF SAID SECTION 12; THENCE SOUTH 02°20'22" EAST, CONTINUING ALONG SAID CENTERLINE, BEING THE WEST LINE OF SAID NORTHWEST QUARTER (NW 1/4) OF THE SOUTHWEST QUARTER (SW 1/4) OF SAID SOUTHWEST QUARTER (SW 1/4), A DISTANCE OF 698.42 FEET TO THE SOUTHWEST CORNER OF SAID NORTHWEST QUARTER (NW 1/4); THENCE NORTH 84°57'31" EAST, DEPARTING SAID CENTERLINE OF JONES BLVD. AND ALONG THE SOUTH LINE OF SAID NORTHWEST QUARTER (NW 1/4), A DISTANCE OF 627.90 FEET TO THE SOUTHEAST CORNER OF SAID NORTHWEST QUARTER (NW 1/4), SAME BEING THE NORTHWEST CORNER OF THE SOUTHEAST QUARTER (SE 1/4) OF THE SOUTHWEST QUARTER (SW 1/4) OF SAID SOUTHWEST QUARTER (SW 1/4); THENCE SOUTH 02°03'53" EAST, ALONG THE WEST LINE OF SAID SOUTHEAST QUARTER (SE 1/4), A DISTANCE OF 693.98 FEET TO THE SOUTHWEST CORNER OF SAID SOUTHEAST QUARTER (SE 1/4) OF THE SOUTHWEST QUARTER (SW 1/4) OF SAID SOUTHWEST QUARTER (SW 1/4) OF SAID SECTION 12, SAME BEING IN THE CENTERLINE OF SAID GRAND TETON DRIVE; THENCE SOUTH 84°32'12" WEST, ALONG THE CENTERLINE OF SAID GRAND TETON DRIVE, 624.80 FEET TO THE POINT OF BEGINNING.

PARCEL II:

THAT PORTION OF THE EAST HALF (E 1/2) OF THE NORTHWEST QUARTER (NW 1/4), AND THAT PORTION OF THE NORTHEAST QUARTER (NE 1/4) AND THAT PORTION OF THE SOUTHEAST QUARTER (SE 1/4) OF SECTION 12 AND THAT PORTION OF THE NORTHEAST QUARTER (NE 1/4) OF THE NORTHEAST QUARTER (NE 1/4) OF SECTION 13 AND BEING ALSO A PORTION OF LOT EIGHTY FOUR (84), AS SHOWN ON A RECORD OF SURVEY ON FILE IN THE CLARK COUNTY, NEVADA, RECORDER'S OFFICE IN FILE 52 OF SURVEYS, AT PAGE 70, TOWNSHIP 19 SOUTH, RANGE 60 EAST, M.D.M., CLARK COUNTY, NEVADA, AND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

LEGAL DESCRIPTION
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JUNE 11, 1998
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BEGINNING AT THE NORTHEAST CORNER OF THE NORTHEAST QUARTER (NE 1/4) OF SECTION 12, SAME BEING THE CENTERLINE INTERSECTION OF IRON MOUNTAIN ROAD, (100 FOOT WIDE ROW) AND DECATUR BLVD., (PROPOSED 120 FOOT WIDE ROW); THENCE SOUTH 00°00'38" EAST, ALONG THE EAST LINE OF SAID NORTHEAST QUARTER (NE 1/4), COINCIDENT WITH THE CENTERLINE OF SAID DECATUR BLVD., 1327.62 FEET; THENCE SOUTH 00°00'18" EAST, CONTINUING ALONG SAID CENTERLINE AND SAID EAST LINE, 1327.47 FEET TO SOUTHEAST CORNER OF SAID NORTHEAST QUARTER (NE 1/4), SAME BEING THE NORTHEAST CORNER OF THE SOUTHEAST QUARTER (SE 1/4) OF SAID SECTION 12; THENCE SOUTH 00°00'41" EAST, ALONG THE EAST LINE OF SAID SOUTHEAST QUARTER (SE 1/4), COINCIDENT WITH SAID CENTERLINE, 1327.50 FEET; THENCE SOUTH 00°00'19" EAST, CONTINUING ALONG SAID EAST LINE AND SAID CENTERLINE, 653.65 FEET; THENCE SOUTH 84°56'38" WEST, DEPARTING SAID EAST LINE AND SAID CENTERLINE, 642.99 FEET; THENCE SOUTH 00°18'59" EAST, 678.18 FEET TO THE SOUTH LINE OF SAID SOUTHEAST QUARTER (SE 1/4) OF SAID SECTION 12, SAME BEING THE NORTH LINE OF THE NORTHEAST QUARTER (NE 1/4) OF THE NORTHEAST QUARTER (NE 1/4) OF SAID SECTION 13 AND BEING ALSO THE NORTH LINE OF SAID LOT 84; THENCE NORTH 84°31'16" EAST, ALONG THE SOUTH LINE OF SAID SOUTHEAST QUARTER (SE 1/4) OF SAID SECTION 12, SAME BEING THE NORTH LINE OF SAID NORTHEAST QUARTER (NE 1/4) OF THE NORTHEAST QUARTER (NE 1/4) OF SAID SECTION 13, ALONG BEING THE NORTH LINE OF SAID LOT 84, A DISTANCE OF 639.72 FEET TO THE SOUTHEAST CORNER OF SAID SOUTHEAST QUARTER (SE 1/4), SAME BEING THE NORTHEAST CORNER OF LOT 84 AND THE NORTHEAST QUARTER (NE 1/4) OF THE NORTHEAST QUARTER OF SAID SECTION 13; THENCE SOUTH 00°10'11" WEST, ALONG THE EAST LINE THE NORTHEAST QUARTER (NE 1/4) OF THE NORTHEAST QUARTER (NE 1/4) OF SAID SECTION 13, AND CROSSING SAID LOT 84, A DISTANCE OF 1329.56 FEET; THENCE SOUTH 00°10'00" WEST, 28.80 FEET TO THE CENTERLINE OF WHISPERING SANDS DRIVE, (PROPOSED 60 FOOT WIDE ROW); THENCE SOUTH 85°06'23" WEST, ALONG THE SOUTH LINE OF SAID LOT 84, COINCIDENT WITH THE SAID CENTERLINE OF WHISPERING SANDS DRIVE, 1250.95 FEET; THENCE NORTH 00°16'49" EAST, 20.92 FEET TO THE SOUTHWEST CORNER OF THE NORTHEAST QUARTER (NE 1/4) OF THE NORTHEAST QUARTER (NE 1/4) OF SAID SECTION 13; THENCE NORTH 00°17'00" EAST, ALONG THE WEST LINE OF SAID NORTHEAST QUARTER (NE 1/4) OF SAID NORTHEAST QUARTER (NE 1/4), A DISTANCE OF 1324.87 FEET TO THE NORTHWEST CORNER OF SAID NORTHEAST QUARTER (NE 1/4), SAME BEING THE SOUTHWEST CORNER OF THE SOUTHEAST QUARTER (1/4) OF THE SOUTHEAST QUARTER (SE 1/4) OF SAID SECTION 12; THENCE NORTH 00°37'01" WEST, ALONG THE WEST LINE OF SAID SOUTHEAST QUARTER (SE 1/4) OF SAID SOUTHEAST QUARTER (SE 1/4), A DISTANCE OF 1344.69 FEET TO THE NORTHWEST CORNER OF THE SOUTHEAST QUARTER (SE 1/4) OF SAID SOUTHEAST QUARTER (SE 1/4), SAME BEING THE SOUTHEAST CORNER OF THE NORTHWEST QUARTER (NW 1/4) OF SAID SOUTHEAST QUARTER (SE 1/4); THENCE SOUTH 85°21'40" WEST, ALONG THE SOUTH LINE OF SAID NORTHWEST QUARTER (NW 1/4), A DISTANCE OF 1262.28 FEET TO THE SOUTHWEST CORNER OF THE NORTHWEST QUARTER (NW 1/4) OF SAID SOUTHEAST QUARTER (SE 1/4); THENCE NORTH 01°12'34" WEST, ALONG THE WEST LINE OF SAID NORTHWEST QUARTER (NW 1/4) OF SAID SOUTHEAST QUARTER (SE 1/4), A DISTANCE OF 680.98 FEET; THENCE NORTH 85°46'17" EAST, DEPARTING SAID WEST LINE, 637.37 FEET; THENCE NORTH 00°54'54" WEST, 676.65 FEET TO THE NORTH LINE OF SAID NORTHWEST QUARTER (NW 1/4) OF SAID SOUTHEAST QUARTER (SE 1/4), SAME BEING THE SOUTH LINE OF NORTHEAST QUARTER OF SAID SECTION 12; THENCE NORTH 00°54'38" WEST, DEPARTING SAID NORTH AND SOUTH LINES, 985.92 FEET;

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THENCE NORTH 86°58'56" EAST, 642.29 FEET; THENCE NORTH 00°36'51" WEST, 367.59 FEET; THENCE SOUTH 86°59'02" WEST, 961.29 FEET; THENCE SOUTH 01°03'25" EAST, 1357.76 FEET TO THE SAID LINE OF SAID NORTHEAST QUARTER (NE 1/4); THENCE SOUTH 86°10'40" WEST, ALONG SAID SOUTH LINE, 323.81 FEET TO THE SOUTHWEST CORNER OF SAID NORTHEAST QUARTER (NE 1/4), SAME BEING THE SOUTHEAST CORNER OF THE EAST HALF (E 1/2) OF THE NORTHWEST QUARTER (NW 1/4) OF SAID SECTION 12; THENCE SOUTH 86°10'45" WEST, ALONG THE SOUTH LINE OF SAID EAST HALF (E 1/2), A DISTANCE OF 1274.77 FEET TO THE SOUTHWEST CORNER OF SAID EAST HALF (E 1/2); THENCE NORTH 01°47'03" WEST, ALONG THE WEST LINE OF SAID EAST HALF (E 1/2), A DISTANCE OF 2758.84 FEET TO THE NORTHWEST CORNER OF SAID EAST HALF (E 1/2), SAME BEING ON THE CENTERLINE OF IRON MOUNTAIN ROAD, (PROPOSED 100 FOOT WIDE ROW); THENCE NORTH 87°44'37" EAST, ALONG THE NORTH LINE OF SAID EAST HALF (E 1/2), COINCIDENT WITH THE CENTERLINE OF SAID IRON MOUNTAIN ROAD, 1301.63 FEET TO THE NORTHEAST CORNER OF SAID EAST HALF (E 1/2) OF THE SAID NORTHWEST QUARTER (NW 1/4), SAME BEING THE NORTHEAST CORNER OF SAID NORTHEAST QUARTER (NE 1/4) OF SAID SECTION 12; THENCE NORTH 87°46'13" EAST, ALONG THE NORTH LINE OF SAID NORTHEAST QUARTER (NE 1/4), COINCIDENT WITH SAID CENTERLINE, 2603.56 FEET TO THE POINT OF BEGINNING.

PARCEL I CONTAINING 174.766 ACRES OF LAND, MORE OR LESS, AS DETERMINED BY COMPUTER METHODS.

PARCEL II CONTAINING 360.150 ACRES OF LAND, MORE OR LESS, AS DETERMINED BY COMPUTER METHODS.

TOTAL AREA OF ZONING PARCEL CONTAINS 534.916 ACRES OF LAND.

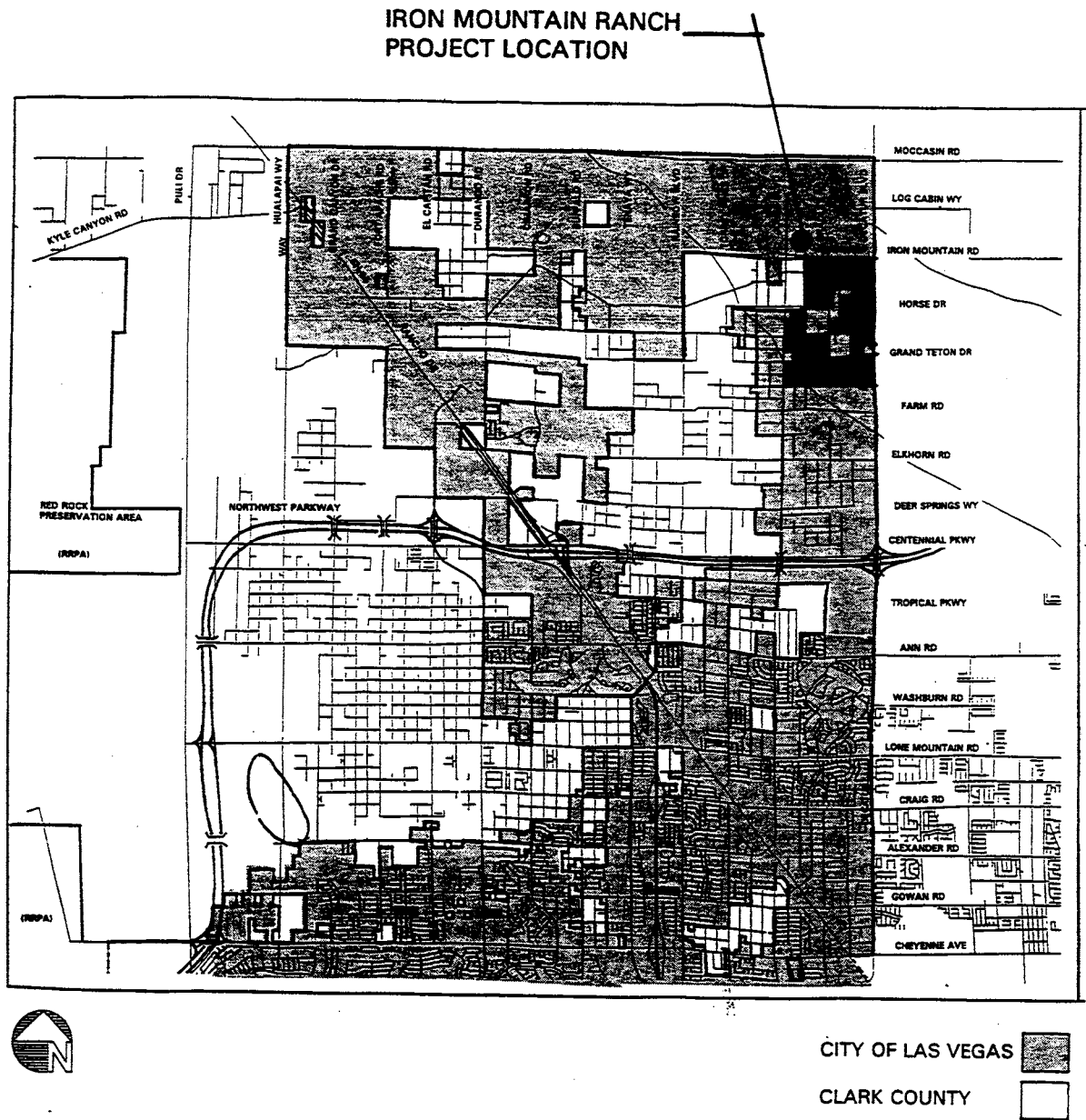
END OF DESCRIPTION.

BASIS OF BEARINGS:

THE BASIS OF BEARINGS FOR THIS PROJECT IS GRID NORTH AS DEFINED BY THE NEVADA COORDINATE SYSTEM OF 1983 (NCS83), EAST ZONE, (2701), AS DETERMINED BY CLARK COUNTY GIS CONTROL POINTS 802, 803 AND 804, AS SHOWN ON A RECORD OF SURVEY ON FILE IN THE CLARK COUNTY, NEVADA, RECORDER'S OFFICE IN FILE 88 OF SURVEYS, AT PAGE 53.

THIS LEGAL DESCRIPTION IS PROVIDED AS A CONVENIENCE AND IS NOT INTENDED FOR THE PURPOSE OF SUBDIVIDING LAND NOT IN CONFORMANCE WITH NEVADA REVISED STATUTES.

REFERENCE 5407-ZONING.LGL



CHAPTER 4 SETTING

4.1 CLARK COUNTY SCHOOL DISTRICT

Based upon the student generation factors established by Clark County School District the proposed project is estimated to generate approximately 527 K-5 students, 245 middle school students, and 290 high school students. School District representatives estimate that these figures translate into need to provide approximately 85% of an elementary school, 15% of a middle school, and 11% of a high school to meet the school needs of the project.

The School District has determined that there are no existing elementary school facilities in the area adequate to serve the project at build out and that the developer will be required to work with the District to identify appropriate means of providing K-5 school facilities as phased project development occurs. The District has determined that the need to house middle school and high school students can be met through existing school facilities located outside of the Iron Mountain Ranch area.

4.2 CITY OF LAS VEGAS FIRE DEPARTMENT

Fire service for Iron Mountain Ranch will be provided by the City of Las Vegas Fire Department from the proposed Station 41 to be located in the northwest area of the city. This facility is funded and is anticipated to be completed within the next 12 - 18 months.

4.3 CITY OF LAS VEGAS METROPOLITAN POLICE DEPARTMENT

Police service for Iron Mountain Ranch will be provided by the Metropolitan Police Department of Las Vegas. The master developer has coordinated with the Department to determine the level of services generated from the Iron Mountain Ranch, with the final determination of the location, capital improvements, and operation of any additional sub-station coordinated through the county's annual comprehensive planning process.

4.4 PARKS / OPEN SPACE

4.4.1 Open Space Calculations (Overall Master Plan)

OPEN SPACE REQUIRED	
Density X .0165 X Gross Acres	30.8
(3.5 X .0165 X 534 = 30.8)	
<hr/>	
OPEN SPACE PROVIDED	30.8
<hr/>	
CITY PARK*	11.0
NEIGHBORHOOD PARKS	10.0
30' MULTI-PURPOSE TRAIL (24' Open Space Credit)	0.7
20' MULTI-PURPOSE TRAIL (14' Open Space Credit)	2.7
25' EQUESTRIAN TRAIL (19' Open Space Credit)	2.4
GREENBELTS	1.0
PARKWAYS (Thematic Loop St's & Trail St's)	3.0
TOTAL	30.8

* City park acreage is in addition to 534 Acre master plan area. For each acre of required open space that the master plan is deficient, one acre of the city park will be developed to city standards by the Master Developer. The ultimate amount of city park acreage developed by the Master Developer is dependent upon the final density approved for the R-PD Master Plan. Changes to the density will result in adjustments to the city park acreage as well.

The master plan provides a comprehensive open space program consisting of the following:

- City Park
- Neighborhood Parks (± 1 Acre each)
- City approved regional trails
- Internal Greenbelts
- Thematic Parkways
- Enhanced Neighborhood Entries

Open Space Calculations (Per Planning Area)

PLANNING AREA	DU/AC	O.S. FACTOR	AC	AC REQ'D	AC. PROVIDED		
	(DU/AC x .0165 x AC = REQ'D AC)				PARK	TRAILS	PARKWAYS / GREENBELT
3	3.49	.0165	35.0	2.0	1.0	0.4	0.35
2	3.46	.0165	28.0	1.6	1.0	0.3	0.15
1	1.9	.0165	8.0	0.2	-	0.1	-
5	4.9	.0165	40.0	3.2	1.0	0.3	0.35
8	1.5	.0165	39.5	1.0	-	1.0	0.05
12	1.5	.0165	29.5	0.7	-	-	0.35
7	1.5	.0165	31.0	0.8	-	0.2	0.3
13	4.0	.0165	40.0	3.2	1.0	-	0.2
11	5.3	.0165	30.0	2.6	1.0	0.2	0.2
10,16	1.6	.0165	30.0	0.8	-	0.2	-
14	2.5	.0165	40.0	1.7	1.0	0.4	0.35
19	3.48	.0165	40.0	2.3	1.0	0.4	0.35
21	5.4	.0165	40.0	3.6	1.0	-	0.5
18	5.0	.0165	40.0	3.3	1.0	-	0.3
20	5.48	.0165	40.0	3.6	1.0	0.6	0.55
15	1.0	.0165	9.0	0.0	-	0.6	-
17	1.7	.0165	14.0	0.4	-	-	-
CITY PARK					11.0	1.1	-
TOTAL				31.0	21.0	5.8	4.0

TOTAL PROVIDED = 30.8 AC.*

- *Notes:
1. Difference between 31.0 Required Acres and the 30.8 Provided Acres is due to rounding.
 2. The ultimate amount of Open Space acreage is dependent upon the final densities achieved in the Tentative Map process, during which final Open Space calculations will be prepared.

4.4.2 City Park

The City Park is located at the southeast corner of Grand Teton Drive and Bradley Road, and is planned as a recreational amenity for residents of the entire northwest area. Please refer to Chapter 5 for a list of permitted uses within the City Park. For each acre of required open space that the master plan is deficient, one acre of the city park will be developed to city standards by the Master Developer. All facilities to be constructed by the Master Developer shall be equestrian related, and shall be in substantial conformance with those previously prepared by Cella Barr Associates titled Final Master Plan 40 Acre Park (dated May 12, 1993) subject to approval by the City of Las Vegas.

The ultimate amount of city park acreage developed by the Master Developer is dependent upon the final density approved for the R-PD Master Plan. Changes to the density will result in adjustments to the city park acreage as well. The park design approval is subject to review and execution of a Parks Agreement between the City of Las Vegas and the Master Developer. Construction of Master Developer park improvements shall commence when 50% of the master plan building permits have been issued.

4.4.3 Neighborhood Parks

Neighborhood park sites are situated throughout the neighborhoods of Iron Mountain Ranch to provide usable smaller scale recreational facilities for neighborhood residents. Each neighborhood park is a minimum of 1 acre, and a series of pedestrian linkages / greenbelts interconnect the various park sites. On average, each 1 acre neighborhood park serves up to 200 residences. The neighborhood parks total 10 acres throughout the master plan area.

All subdivisions with 10,000 square foot lots or smaller feature either of the following:

- A shared park facility between two subdivisions with a combined total of 1 acre of usable park area.
- An individual park within the subdivision, providing a minimum of 1 acre of usable park area.

All such parks must provide at least three of the permitted uses for neighborhood parks as listed in Table 5.1.2. Parking areas and restrooms shall not be counted toward this minimum requirement.

4.4.4 City Approved Regional Trails

The master plan incorporates the city approved regional trail system as an important linkage between Iron Mountain Ranch and the surrounding northwest area as further described in Section 6.4. The acreage devoted to these trail systems, excluding required R.O.W.'s, shall be counted toward the open space requirement as detailed in Section 4.4.1, Open Space Calculations.

4.4.5 Internal Greenbelts

The internal greenbelts provide important linkages among neighborhood and community amenities as well as regional amenities, such as the city approved trails and vacant BLM parcel at the southwest corner of Horse Drive and Bradley Road. These 20' greenbelt corridors will consist of 5' meandering pathways with the remaining area to be landscaped. Please refer to Exhibit 7.10.

4.4.6 Enhanced Neighborhood Entries

Each entry into individual neighborhoods within Iron Mountain Ranch shall feature and enhanced landscape treatment as further described in Section 7.9.4.

4.5 UTILITIES PLAN

Utility services to Iron Mountain Ranch will be provided in conformance with the Northwest General Plan Amendment and City of Las Vegas requirements. Extensions of services and sizes will be determined during the improvement plan stage. Fire protection, electric and telephone services will be coordinated with the respective agencies.

Please refer to the Appendix for the planned phasing of infrastructure including street improvements, water and sewer service. Storm drainage and flood control plans will be developed in accordance with the City of Las Vegas standards in conduction with the preparation of street improvement plans.

CHAPTER 5 LAND USES

5.1 PERMITTED USES

5.1.1 Residential

USE	4,500 S.F. LOTS	7,000 S.F. LOTS	10,000 S.F. LOTS	20,000 S.F. LOTS	30,000 S.F. LOTS
Patio Covers	P	P	P	P	P
Residential Care Facilities	P	P	P	P	P
Single Family Detached Dwellings	P	P	P	P	P
Bed / Breakfast Inn				S	S
Caretaker Quarters / Domestic or Security Unit				C	C
Guest Quarters / Secondary Living Unit				C	C
Home Occupation	H	H	H	H	H
Horse Corral or Stable				C	C

5.1.2 Open Space

USE	CITY PARK	NEIGHBORHOOD PARKS	GREENBELTS
Baseball Fields	P		
Soccer Fields	P		
Open Play Areas	P	P	
Basketball / Multi-Purpose Courts	P	P	
Tennis Courts	P	P	
Barbecue Areas	P	P	
Picnic Areas	P	P	
Tot Lots	P	P	P
Trails	P	P	P
Swimming Pools / Spas	P	P	
Parking	P	P	
Trellis / Shelters	P	P	P
Restrooms	P	P	
Horse Corrals, Stables, Arena, & Related Equestrian Facilities	P		

P=Permitted; S=SUP Permit; C=Permitted with Conditions (CLV Code, Chapter 19A.04.060); H=Home Occupation Permit

5.2 PROHIBITED USES

The uses permitted in this Chapter are classified on the basis of common operational characteristics and land use compatibility. Uses not specifically listed in this Chapter are prohibited. However, additional new and unlisted uses may be permitted by the Director of Planning and Development if the Director finds that the use is similar to other uses within the same land use designation.

CHAPTER 6

CIRCULATION

6.1 MASTER PLAN STREETS

Primary access points to Iron Mountain Ranch will be provided by the following roads, based upon the City of Las Vegas Public Street Standards. Rural street improvements may be allowed on Horse Drive and Bradley Road north of Grand Teton Drive subject to the approval of the Master Drainage Study and Master Traffic Access Analysis Report allowing such. Please refer to Exhibits 6.1 through 6.11 for the standard street cross-sections.

- Grand Teton Drive (120' R.O.W)
- Decatur Boulevard (100' R.O.W)
- Jones Boulevard (80' R.O.W.)
- Horse Drive (80' R.O.W.)
- Iron Mountain Road (60' R.O.W)
- Bradley Road (60' R.O.W.)

Builders within Iron Mountain Ranch shall be responsible to participate in a traffic signal contribution program as jointly established by the City of Las Vegas and builders / developers in the northwest area.

Please refer to the Appendix for the planned phasing of infrastructure including street improvements.

6.2 NEIGHBORHOOD STREETS

6.2.1 Thematic Loop Streets

Most villages feature a themed loop street which connects two adjacent neighborhoods. The landscape treatment of the loop streets will reinforce the importance of these neighborhood linkages through the use of a thematic street tree program and a 5' landscaped parkway separating the sidewalk from the back of curb. The parkway will be planted with low-maintenance groundcover plant material.

The cross section for the loop street is 40' measured to the back of standard 30" Roll-curbs, plus a 5' parkway and 5' sidewalk on each side. The common element will be granted to the homeowner's association for the on-going maintenance of the parkways. Irrigation and power supply for the parkway landscape will be taken from

the neighborhood park within the subdivision. Instead of the standard 30" Roll-curbs, builders will be permitted to use 24" L-curbs subject to approval by the Master Developer and city planning and public works staff. Please refer to Exhibit 6.6.

6.2.2 Residential Trail Street

Some neighborhoods feature a residential trail street, which provides a secondary "themed" streetscape within the subdivision. The cross-section for these streets consist of 36' measured to the back of curbs, plus a 5' parkway and 5' sidewalk on each side. The common element will be granted to the homeowner's association for the on-going maintenance of the parkways. Irrigation and power supply for the parkway landscape will be taken from the neighborhood park within the subdivision. Instead of the standard 30" Roll-curbs, builders will be permitted to use 24" L-curbs subject to approval by the Master Developer and city planning and public works staff. Please refer to Exhibit 6.7.

6.2.3 Standard Residential Streets

The remaining streets within each neighborhood use a modified grid pattern with a standard 44' R.O.W. cross-section consisting of 36' measured to the back of standard 30" Roll-curbs, with an attached 4' sidewalk back of curb on each side. In place of the standard 30" Roll-curbs, builders will be permitted to use 24" L-curbs, subject to approval by the Master Developer and city planning and public works staff. Please refer to Exhibit 6.8.

6.2.4 Private Drives

The 30,000 S.F. lots in Planning Area 1 along Jones Boulevard feature a private driveway providing access to each of the lots on these perimeter conditions adjacent to Desert Rural neighbors. The private drive will consist of 28' pavement with 2' decomposed granite shoulders on each side subject to approval by the City of Las Vegas Fire Department and approval of the Traffic Access Analysis Report. These private drives will enhance the rural character of the area and provide a greater setback to the face of the residences. A split rail fence compatible with the overall community theme will be used in the parkway area between the perimeter roads and private drives to further enhance the neighborhood transition. The private drive and landscape area will be maintained by the homeowner's association. Please refer to Exhibit 6.9.

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6.2.5 Local Perimeter Streets

Local residential perimeter streets will provide direct access to the 30,000 square foot, 20,000 square foot and 10,000 square foot buffer lots in Planning Areas 1, 9, 10, 15, 16, and 17, including Whispering Sands Drive, Thom Boulevard, Racel Street, Unicorn Street, Brent Lane and Bradley Road.

6.3 R.O.W. / EASEMENT DEDICATIONS**6.3.1 Streets**

The following street R.O.W.'s will be dedicated to the City of Las Vegas. Street sections may be reduced subject to approval of the Traffic Access Analysis Report and the Master Plan of Streets and Highways as amended.

- Grand Teton Drive: 60' dedication adjacent to property
- Grand Teton Drive: 120' dedication within the property

- Decatur Boulevard: 50' dedication adjacent to property

- Jones Boulevard: 40' dedication adjacent to property

- Horse Drive 40' dedication adjacent to property
- Horse Drive: 80' dedication within the property

- Iron Mountain Road: 30' dedication adjacent to property

- Bradley Road: 30' dedication adjacent to property
- Bradley Road: 60' dedication within the property

- Whispering Sand Drive: 30' dedication adjacent to property

- Thom Boulevard: 25'-6" dedication adjacent to property
- Thom Boulevard: 51' dedication within the property

- Racel Street: 25'-6" dedication adjacent to property

- Unicorn Street: 25'-6" dedication adjacent to property

- Brent Lane: 25'-6" dedication adjacent to property

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- Brent Lane: 51' dedication within the property
 - Neighborhood Streets*

Thematic Loop Streets	60' dedication with 10' maintenance easement each side to H.O.A. (sidewalk and parkway)
Residential Trail Streets	56' dedication with 10' maintenance easement each side to H.O.A. (sidewalk and parkway)
Standard Res. Streets	44' dedication within the property
- * If individual builders propose to gate their subdivision, then streets within that subdivision would remain private and not be dedicated to the City of Las Vegas. Private streets will be owned and maintained by an H.O.A. established for that subdivision.

6.3.2 City Approved Regional Trails

The following trails will be dedicated to the City of Las Vegas:

- 30' Multi-Purpose Trail adjacent to the east side of Bradley Road, south of Grand Teton Drive.
- 25' Equestrian Trail adjacent to the north side of Grand Teton Drive and along the east side of Bradley Road north of Grand Teton Drive.
- 20' Multi-Purpose Trail adjacent to the south side of Grand Teton Drive and along the west side of Bradley Road north of Grand Teton Drive.

6.3.3 Easements

Easements for the transmission of utilities and other services will be identified during the mapping process. Dedications to the appropriate entities will be determined at that time.

6.4 CITY / REGIONAL TRAILS

6.4.1 City Approved Regional Trails

The master plan incorporates the city approved regional trail system as an important linkage between Iron Mountain Ranch and the surrounding northwest area. The acreage devoted to these trail systems, exclusive of required R.O.W., shall be counted toward the open space requirement as further detailed in Section 4.4.1, Open Space Calculations.

The following trails will be built as a part of project development:

30' Multi-Purpose Trail

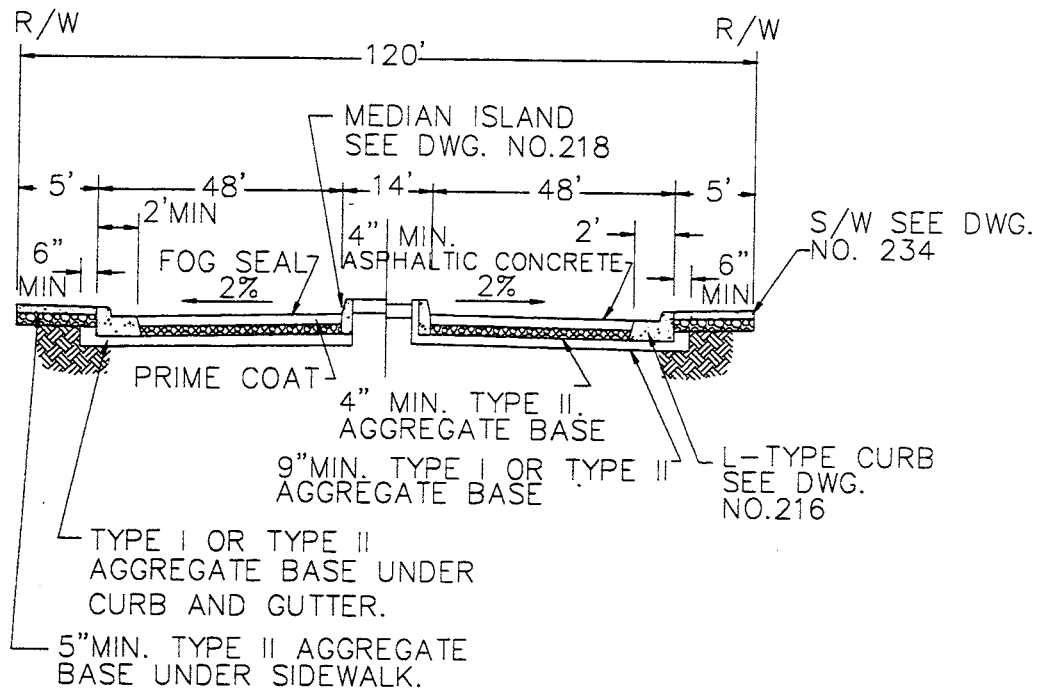
The 30' multi-purpose trail is located along the west side of Bradley Road north of Grand Teton Drive, and along the east side of Bradley Drive adjacent to the city park south of Grand Teton. This trail will be developed within the master plan area in accordance with city requirements, consisting of a 7½' pedestrian trail and a 12½' equestrian trail separated by a 6' landscape area. The remaining portions of the 30' corridor will be landscaped. Please refer to Exhibit 7.6.

20' Multi-Purpose Trail

Located along the south side of Grand Teton Drive and the west side of Bradley Road north of Grand Teton Drive, a 20' multi-purpose trail will be provided within the master plan area in accordance with city standards. This trail will consist of a meandering 7½' wide multi-purpose trail (equestrian excluded), with the remaining area to be landscaped. Please refer to Exhibit 7.8.

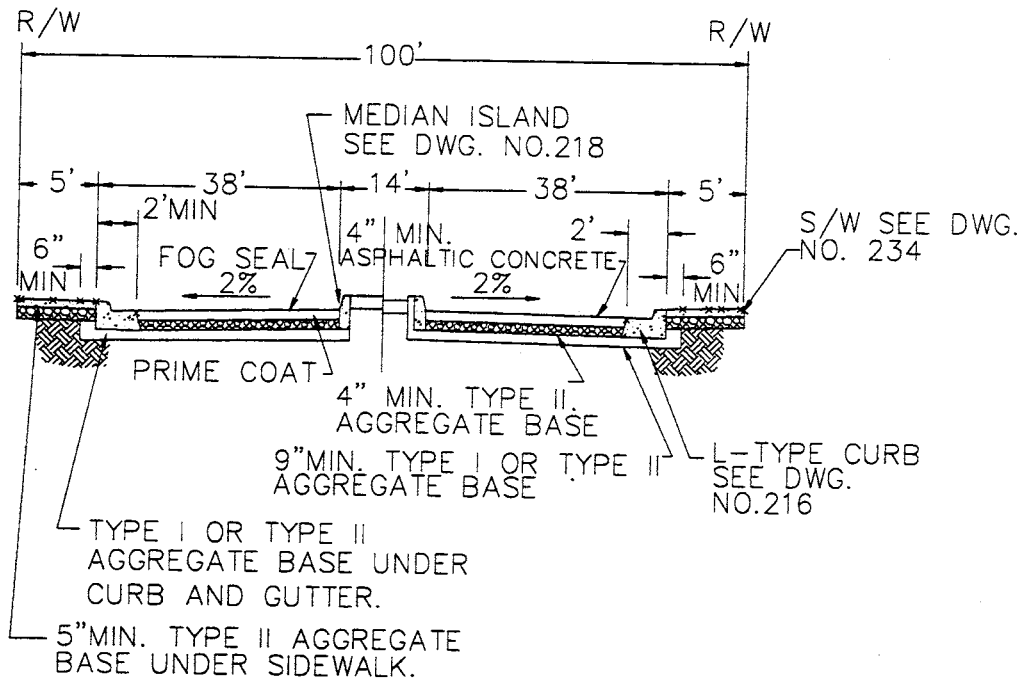
25' Equestrian Trail

The 25' equestrian trail is located along the north side of Grand Teton Drive and the east side of Bradley Road north of Grand Teton, and will be developed within the master plan area in accordance with city standards. This trail will consist of a 12½' wide equestrian trail with the remaining area to be landscaped. Please refer to Exhibit 7.7.



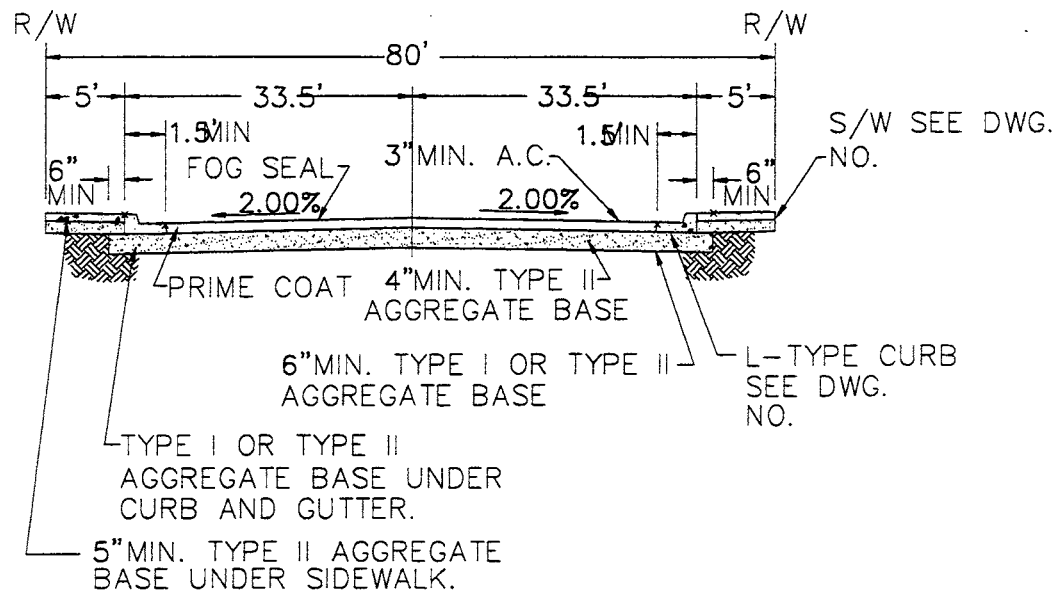
NOTES:

1. A.C. PAVEMENT TO BE 1/2" MAXIMUM ABOVE LIP OF GUTTER AFTER COMPACTION EXCEPT FOR GUTTER WITH DEPRESSED LIP.
2. THE GRADE BREAK OCCURRING IN THE CROSS SECTION SHALL FALL BETWEEN DRIVING LANES.
3. STRUCTURAL SECTION SHOWN IS BASED ON A SUBGRADE "R" VALUE OF 20. OTHER STRUCTURAL SECTIONS MAY BE REQUIRED BASED ON ENGINEERING ANALYSIS OF "R" OR "CBR" VALUES DETERMINED BY SOIL TESTING.



NOTES:

1. A.C. PAVEMENT TO BE 1/2" MAXIMUM ABOVE LIP OF GUTTER AFTER COMPACTION EXCEPT FOR GUTTER WITH DEPRESSED LIP.
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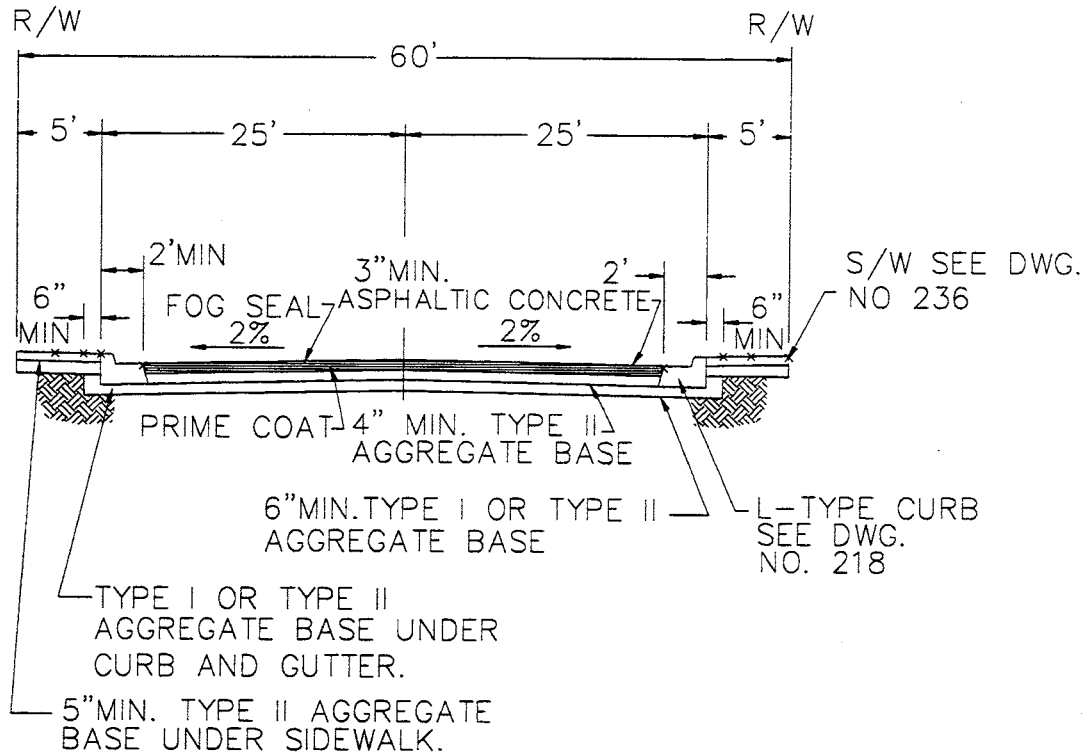
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DESIGN GUIDELINES

Exhibit 6.3
JONES BOULEVARD, HORSE DRIVE
ARTERIAL w/out MEDIAN ISLAND



NOTES:

1. A.C. PAVEMENT TO BE 1/2" MAXIMUM ABOVE LIP OF GUTTER AFTER COMPACTION.
2. THE GRADE BREAK OCCURING IN THE CROSS SECTION SHALL FALL BETWEEN DRIVING LANES.
3. STRUCTURAL SECTION SHOWN IS BASED ON A SUBGRADE "R" VALUE OF 20. OTHER STRUCTURAL SECTIONS MAY BE REQUIRED BASED ON ENGINEERING ANALYSIS OF "R" OR "CBR" VALUES DETERMINED BY SOIL TESTING.

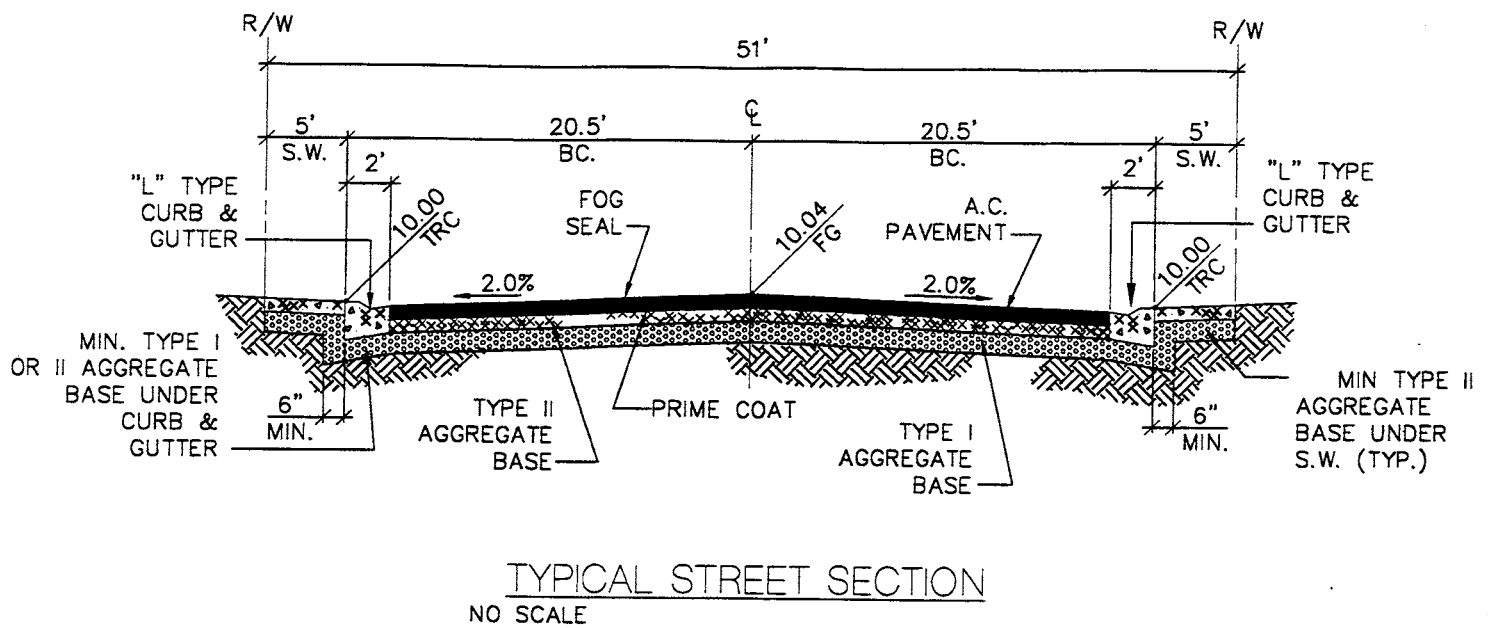
Exhibit 6.4

**IRON MOUNTAIN RD.,
BRADLEY DR., WHISPERING SANDS DR.**

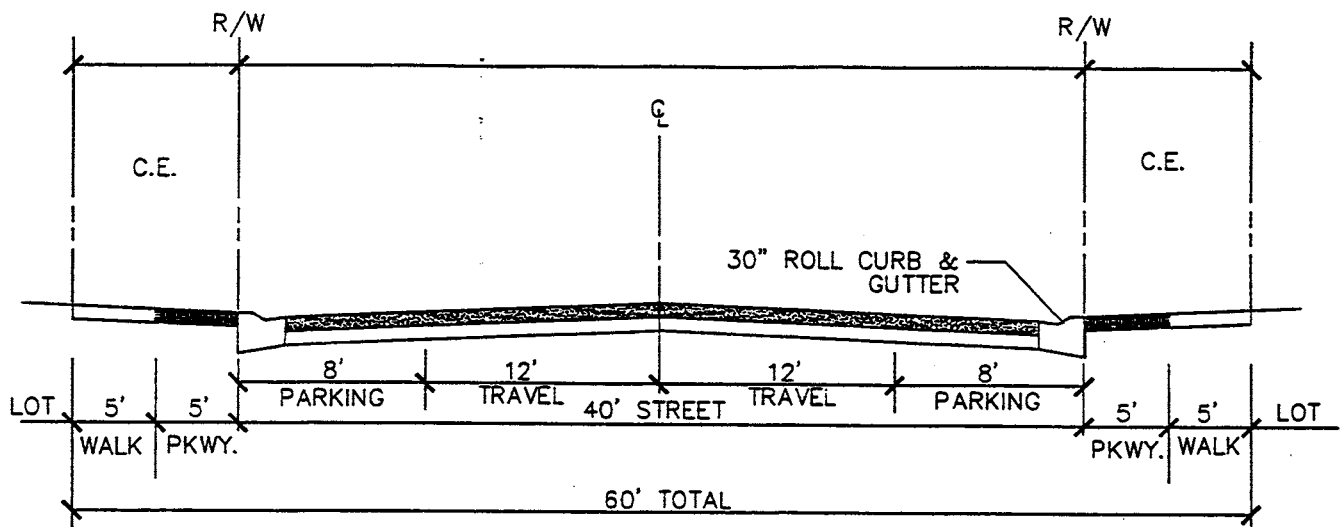
**NON-RESIDENTIAL LOCAL OR
MINOR RESIDENTIAL COLLECTOR**

Iron Mountain Ranch

DESIGN GUIDELINES

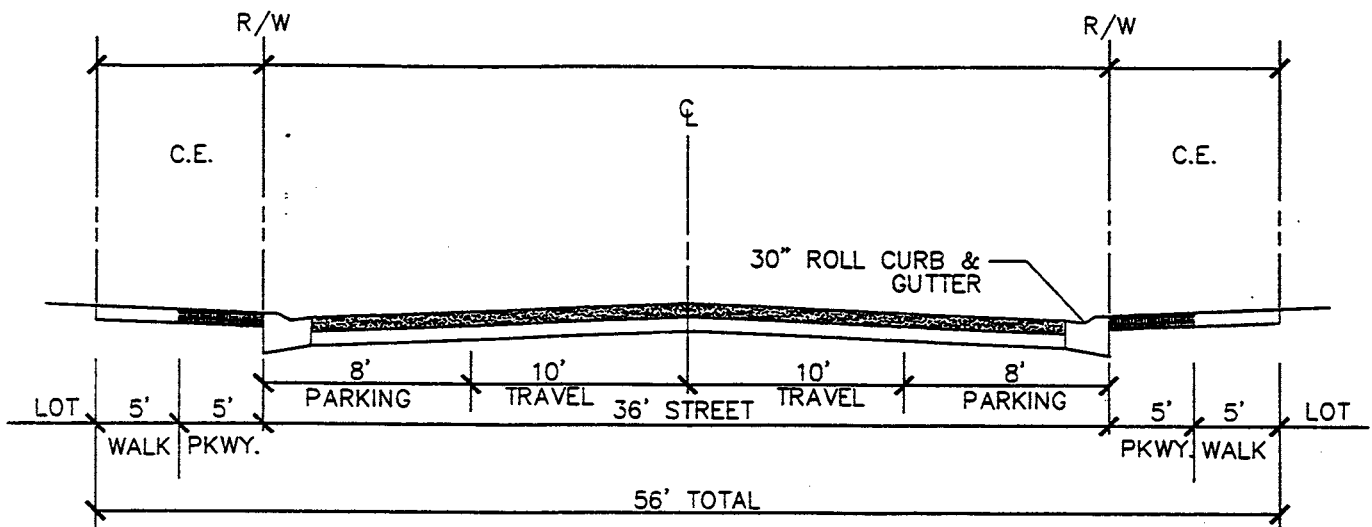
**Exhibit 6.5**

**BRENT LN., UNICORN ST.,
RACEL ST., THOM BLVD.**



THEMATIC LOOP STREET
LEVEL 1 EXHIBIT 6.6 NO SCALE

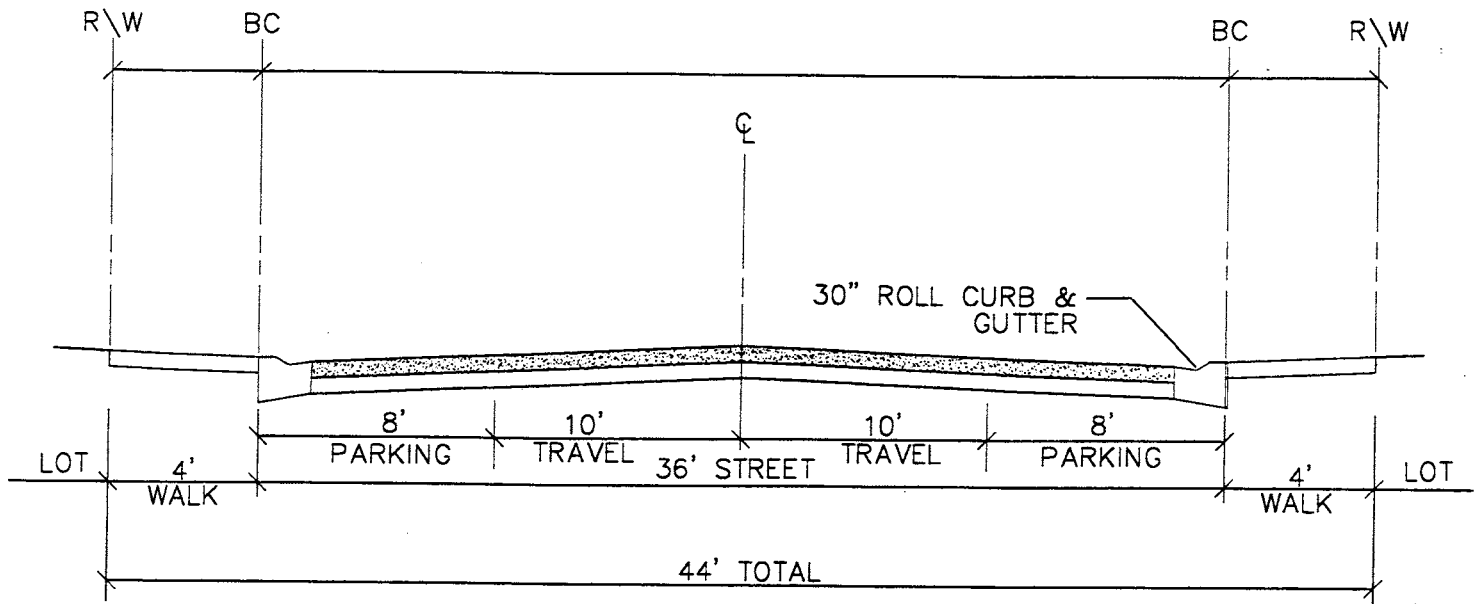
- NOTE: 1. THE COMMON ELEMENT (C.E.) WILL BE GRANTED TO THE HOMEOWNER'S ASSOCIATION FOR THE ON GOING MAINTENANCE OF THE PARKWAYS
2. IN PLACE OF THE STANDARD 30-INCH ROLL-CURB, BUILDERS WILL BE PERMITTED TO USE 24-INCH L-CURB, SUBJECT TO APPROVAL BY THE MASTER DEVELOPER AND CITY PLANNING AND PUBLIC WORKS STAFF.



RESIDENTIAL TRAIL STREET

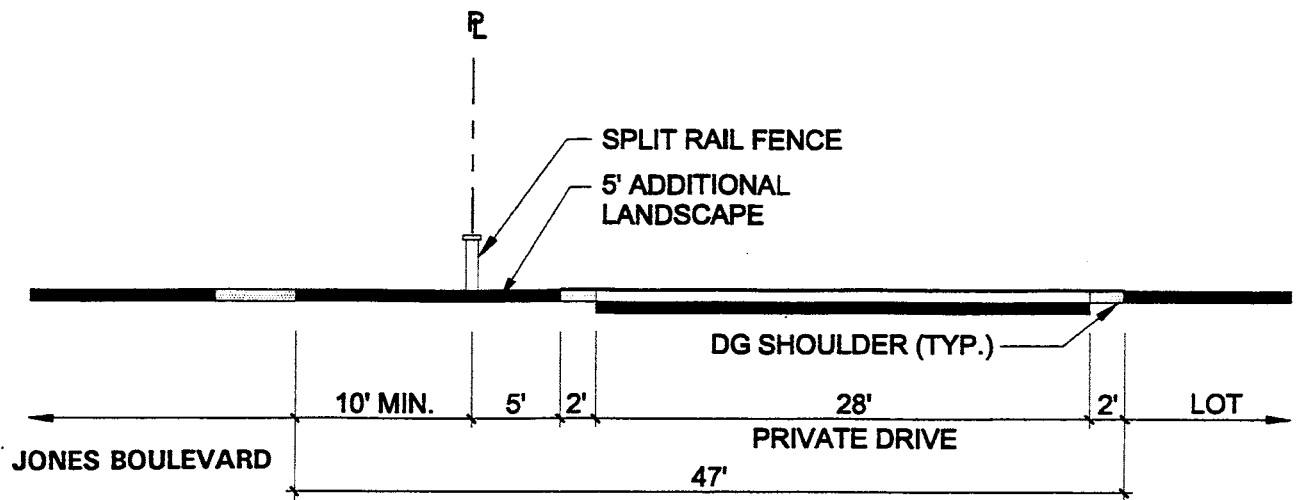
LEVEL 2 EXHIBIT 6.6A NO SCALE

- NOTE: 1. THE COMMON ELEMENT (C.E.) WILL BE GRANTED TO THE HOMEOWNER'S ASSOCIATION FOR THE ON GOING MAINTENANCE OF THE PARKWAYS
2. IN PLACE OF THE STANDARD 30-INCH ROLL-CURB, BUILDERS WILL BE PERMITTED TO USE 24-INCH L-CURB, SUBJECT TO APPROVAL BY THE MASTER DEVELOPER AND CITY PLANNING AND PUBLIC WORKS STAFF.

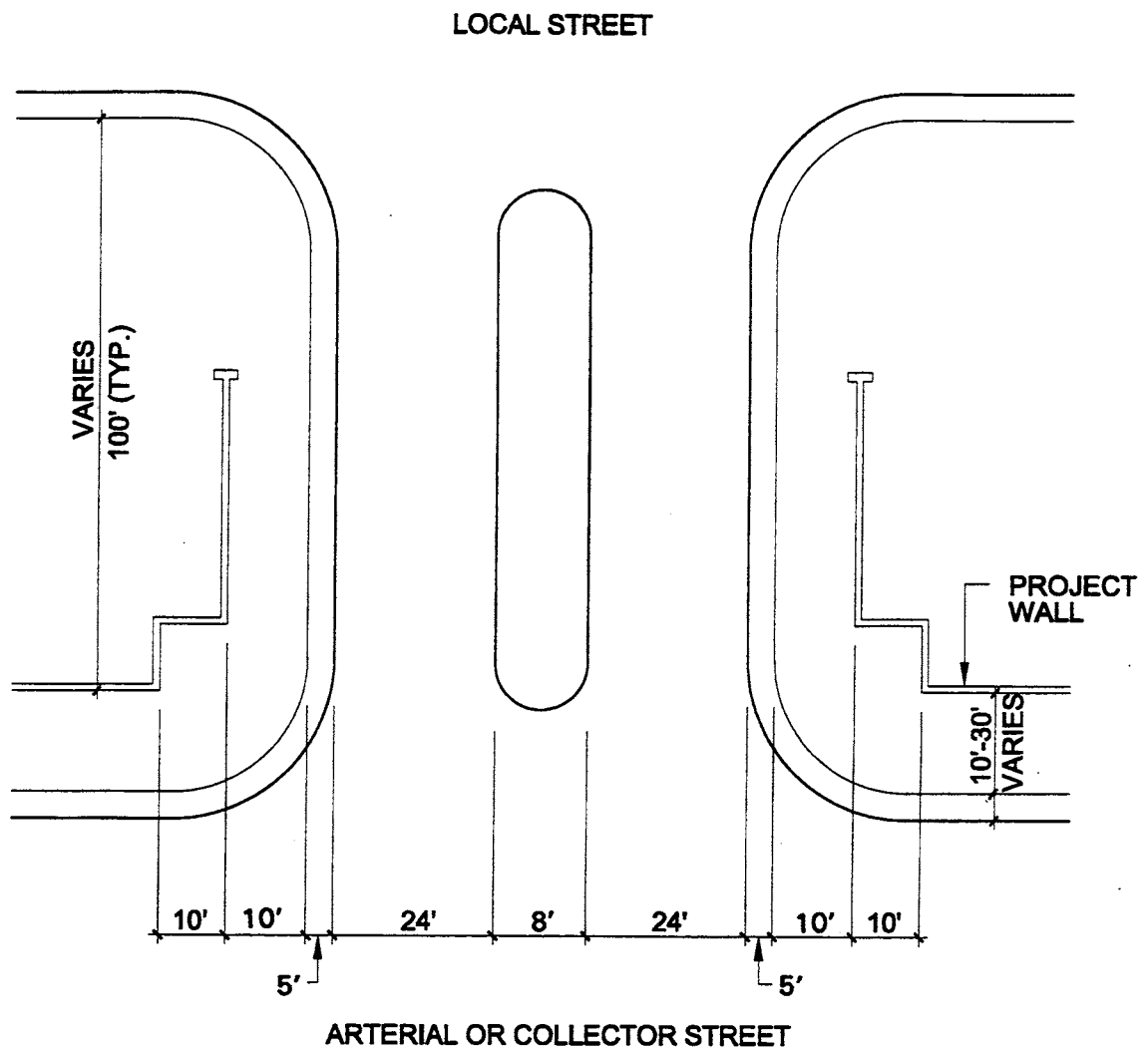


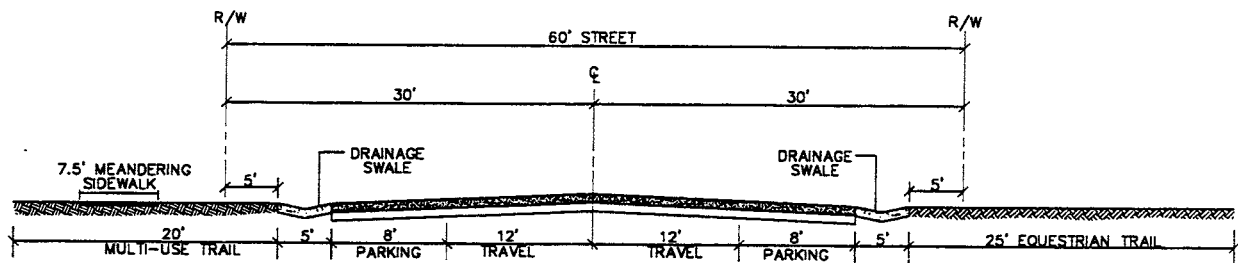
RESIDENTIAL STREET
LEVEL 3 EXHIBIT 6.7 NO SCALE

NOTE: 1. IN PLACE OF THE STANDARD 30-INCH ROLL-CURB, BUILDERS WILL BE PERMITTED TO USE 24-INCH L-CURB, SUBJECT TO APPROVAL BY THE MASTER DEVELOPER AND CITY PLANNING AND PUBLIC WORKS STAFF.



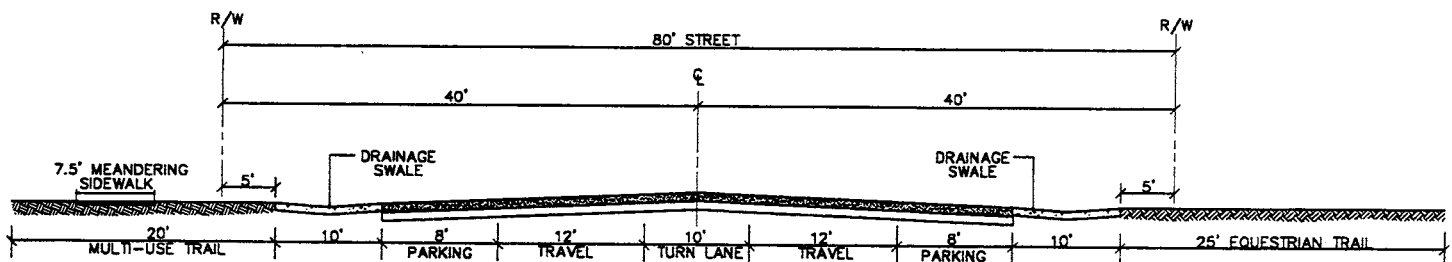
NOTE: 1. PARALLEL PARKING ON ONE SIDE OF
PRIVATE ACCESS DRIVE ONLY



**BRADLEY ROAD**

NO SCALE

NOTE: APPROVAL OF THIS STREET SECTION IS
DEPENDENT UPON MEETING HYDROLOGY
STREET CRITERIA

**HORSE DRIVE**

NO SCALE

NOTE: APPROVAL OF THIS STREET SECTION IS
DEPENDENT UPON MEETING HYDROLOGY
STREET CRITERIA

CHAPTER 7

DEVELOPMENT STANDARDS

7.1 ARCHITECTURE DEVELOPMENT STANDARDS MATRIX

NEIGHBORHOOD THEME	
Each subdivision shall feature one of these architectural themes as further described in Section 7.2:	Ranch Mediterranean Desert Contemporary
FLOOR PLAN / ELEVATION OPTIONS	
Floor Plans	3 Floor plans per subdivision (Min.)
Elevation Options	3 Elevation options per Floor Plan (Min.) (Identical elevations, including reverse images, shall not be plotted adjacent to each other)
BUILDING MASSING AND SCALE	
Single Story Elements: (Please refer to Exhibit 7.01)	1) At least 20% of the dwellings within a subdivision must be single story; or 2) At least 20% of the dwellings within a subdivision must have a building mass which combines single story and two story building forms; or 3) A combination of 1) and 2) above totalling at least 20% of the dwellings within the subdivision.
Staggered Wall Planes (One Story Units) (Please refer to Exhibit 7.02)	No more than 60% of the front elevation can be composed of a single wall plane. (Minimum vertical or horizontal offset is 2')
Staggered Wall Planes (Two Story Units) (Please refer to Exhibit 7.02)	No more than 40% of the front elevation can be composed of a single wall plane. (Minimum vertical or horizontal offset is 2')
Garage plane forward of living area	Maximum 50% of the units within a subdivision
One or combination of the following: <ul style="list-style-type: none"> ▪ Living Area forward of the garage plane a minimum of 2' ▪ Side entry garage ▪ Front Porch 36 Sq. Ft. (Min.) 4' Min. Dimension ▪ Front Courtyard 70 Sq. Ft. (Min.) Enclosed on 3 sides 	Minimum 50% of the units within a subdivision
Main Residence Entry on Side Elevation	Maximum 40% of the units within a subdivision

WINDOW AND DOOR OPENINGS**Front Elevations**

At least one principal window recessed into thickened walls or projected forward of the wall plane
(Min. Recess or Projection is 6")

And one of the following:

- Extended roof overhangs at principal windows
(Minimum Overhang: 12")
- Trim surrounds, headers, or sills at all windows
(Min. Trim Material: 2" x 6")
- Shutters at principal windows

All 1st story
and 2nd story windows forward of
the return wall on side elevations,
and all 2nd story rear or side
elevations which abut interior
streets, community open space or
perimeter roadways

One of the following is required:

- A principal window recessed into thickened walls
or projected forward of the wall plane (Min.
Recess or Projection 6")
- Extended roof overhangs at principal windows
(Minimum Overhang: 12")
- Trim surrounds, headers, or sills at all windows.
(Minimum Trim Material 2" x 6")
- Shutters at Principal windows

BUILDING MATERIAL & COLOR BLOCKING**Interior Lots**

Material and color blocking shall wrap the outside corner of the
building a minimum of 3'.

Elevations which are visible from abutting
streets, R.O.W.'s, or open space

Material blocking shall wrap the outside corner of the building a
minimum of 3'.

Color blocking, including window trims, shall wrap the outside
corner and shall extend the entire length of the elevation.

Columns and Porches

Shall be wrapped in their entirety.

COLOR SCHEMES**Number of color schemes required:**

4 color schemes (min.) per subdivision. Each color scheme
shall have at least 3 colors, including:

(Refer to Exhibits 7.08 and 7.9 showing an array of
acceptable base, accent and trim colors, which
complement the *Desert Harmony* theme of Iron
Mountain Ranch. Color selections must be
consistent with the palette of colors shown, and are
subject to review and approval by the master
developer and city planning staff.)

- Base Color 70% Maximum
 - Accent 5% Minimum
 - Trim 10% Minimum
- 15% (Remaining may be any
combination of above.)

ROOFS

Main Roof Form	% of Units
Side / Side Gable	25% Min.
Front / Rear Gable	25% Min.
Hip	25% Min.
	25% (Remaining units may be any of the above)
Total	100%
Roof Pitch	4:12 to 8:12
Roof Overhang	Roof eaves shall have a minimum of 12" overhang; or may be reduced to a 6" overhang when a corresponding fascia element of 12" or greater is provided.
	Rakes may be tight or have a 12" overhang as appropriate to the architectural style of the residence.
Roof Material (Please refer to Exhibit 7.10)	Concrete or Clay Tiles <ul style="list-style-type: none"> ■ Flat ■ Shake-like ■ Barrel
Roof Finish	Matte finish to minimize glare
Skylights	Framing material shall be colored to match the adjoining roof.
	White dome skylights are not permitted.
Mechanical Equipment	Mechanical equipment such as air conditioners, heaters, evaporative coolers, and other such devices are not permitted on the roof.
	Mechanical devices such as exhaust fans, vents and pipes shall be painted to match the adjoining roof surfaces.

7.2 ARCHITECTURAL THEME

The architectural theme for residential neighborhoods within Iron Mountain Ranch shall reinforce the "*Desert Harmony*" theme of the overall community. Architectural styles which are compatible with the desert setting of Iron Mountain Ranch include Ranch, Mediterranean, or Desert Contemporary styles. A brief description of each style is provided below:

Ranch

Distinguishing characteristics of Ranch style architecture include:

- Horizontal emphasis of building forms.
- Low character enhanced through the use of low plate lines and low-pitched roofs.
- Frequently feature covered front porches, often covered with a broken-pitch roof.
- Flat shake-like concrete roof tiles.
- Use of materials and details providing a rustic appearance.
- Use of desert tone colors to reinforce the desert setting.

Mediterranean

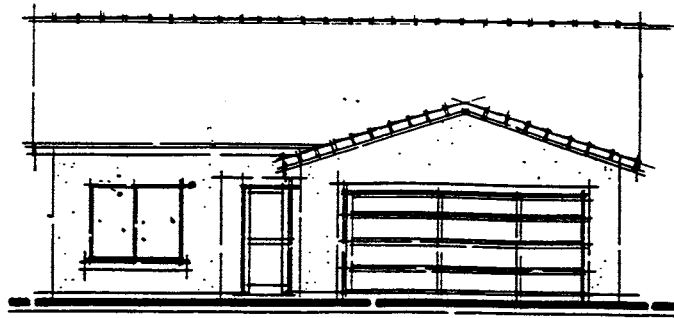
The Mediterranean style of architecture is characterized by the following:

- Asymmetrical building forms, often combining one and two-story elements.
- Often feature tower elements to accentuate building entries.
- Roofs are typically low-pitched and feature red or multi-color barrel tiles.
- Special accent features such as a prominent arch placed above an entry door or principal window.
- Decorative use of detail elements such as shutters, grille work, clay vents, etc.

Desert Contemporary

Characteristics which distinguish the Desert Contemporary architectural style include:

- Strong building massing and forms, often accentuating a horizontal character.
- Extension of building walls into the landscape as sculptural elements.
- Use of offset wall planes to create strong shadow lines.
- Low pitched roof forms, covered with flat concrete roof tiles.
- Limited ornamentation

**SINGLE STORY**

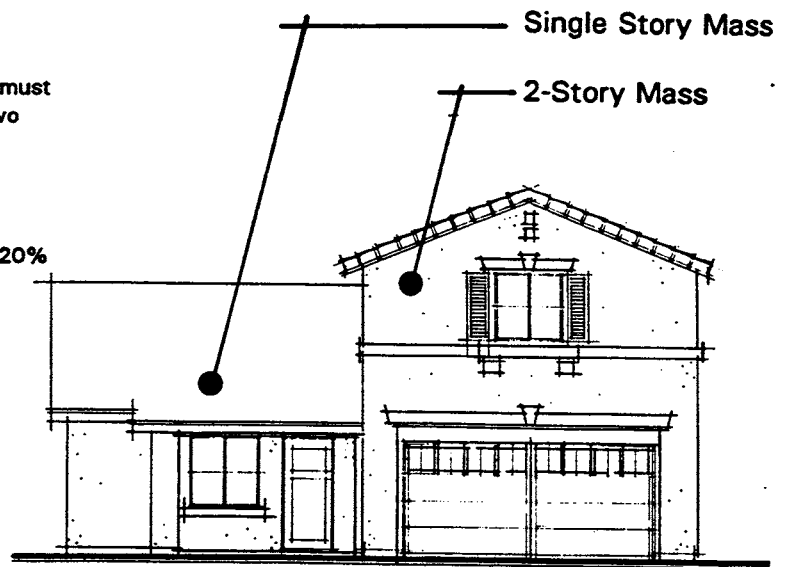
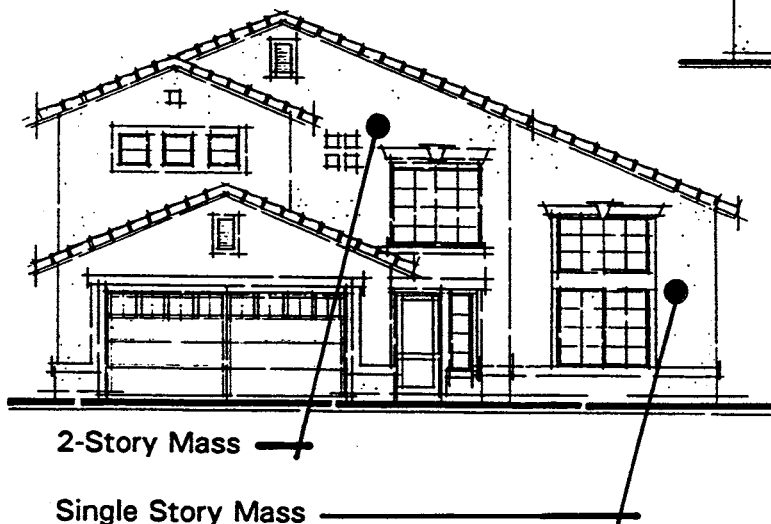
1) At least 20% of the dwellings within a subdivision must be single story;

or

2) At least 20% of the dwellings within a subdivision must have a building mass which combines single story and two story building forms;

or

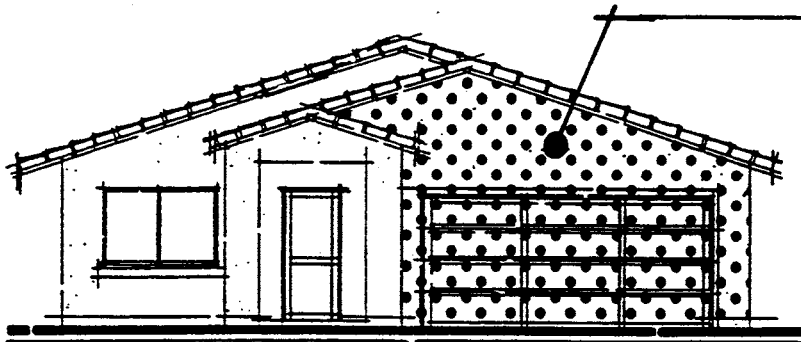
3) A combination of 1) and 2) above totalling at least 20% of the dwellings within the subdivision.

**COMBINATION
SINGLE STORY & TWO STORY**

Iron Mountain Ranch

DESIGN GUIDELINES

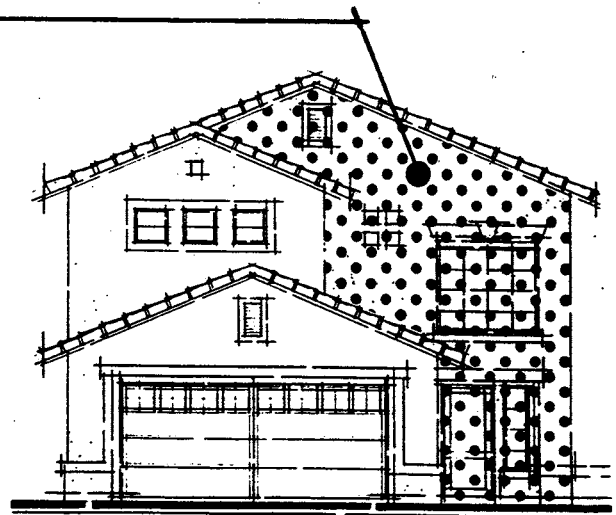
Exhibit 7.01
**BUILDING MASSING
SINGLE-STORY ELEMENTS**



SINGLE STORY UNITS
No more than 60% of the front elevation can be composed of a single wall plane.

SINGLE STORY

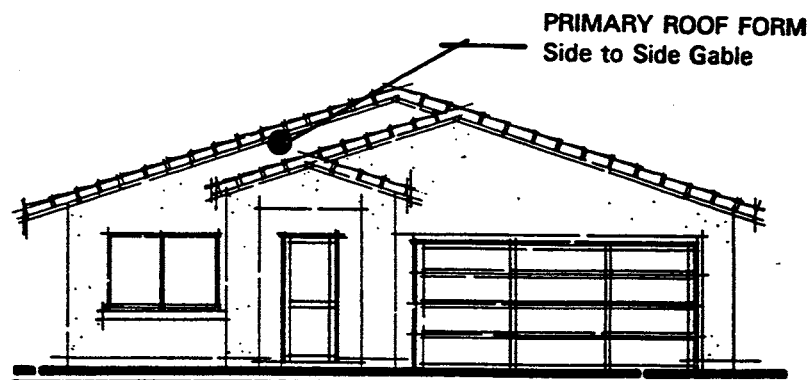
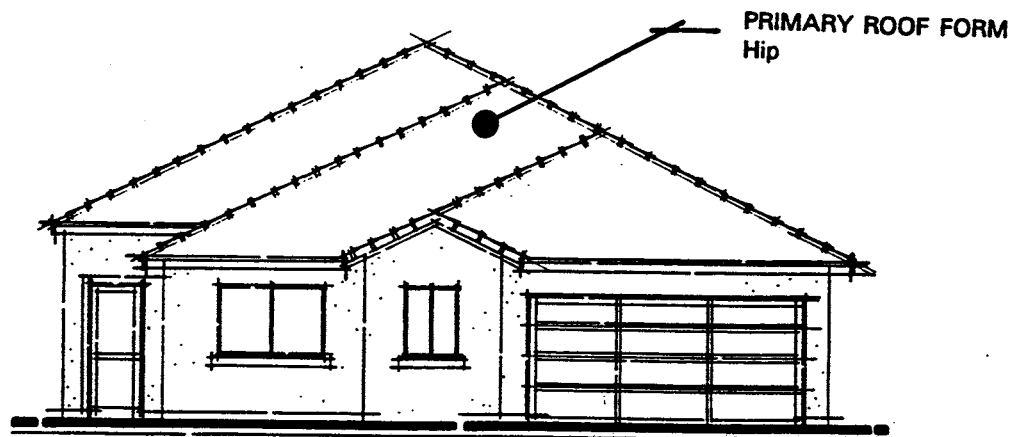
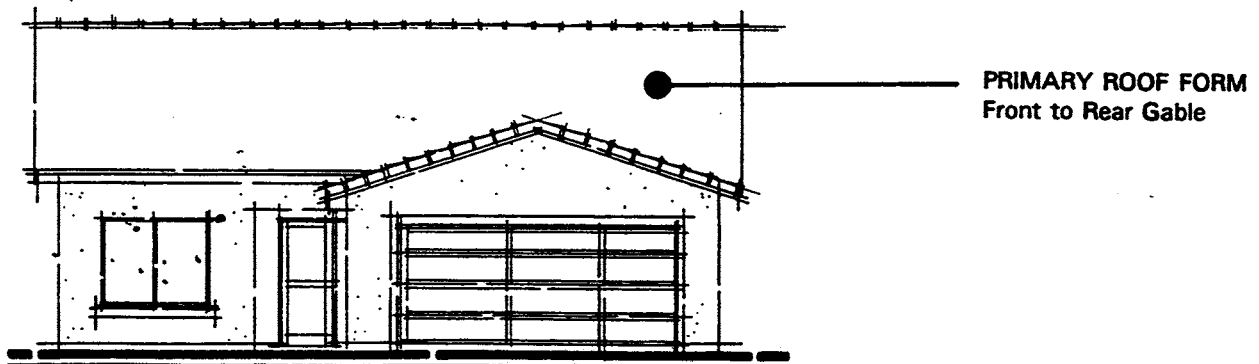
TWO STORY UNITS
No more than 40% of the front elevation can be composed of a single wall plane.



TWO STORY

NOTE

Minimum vertical or horizontal offset is 2'.



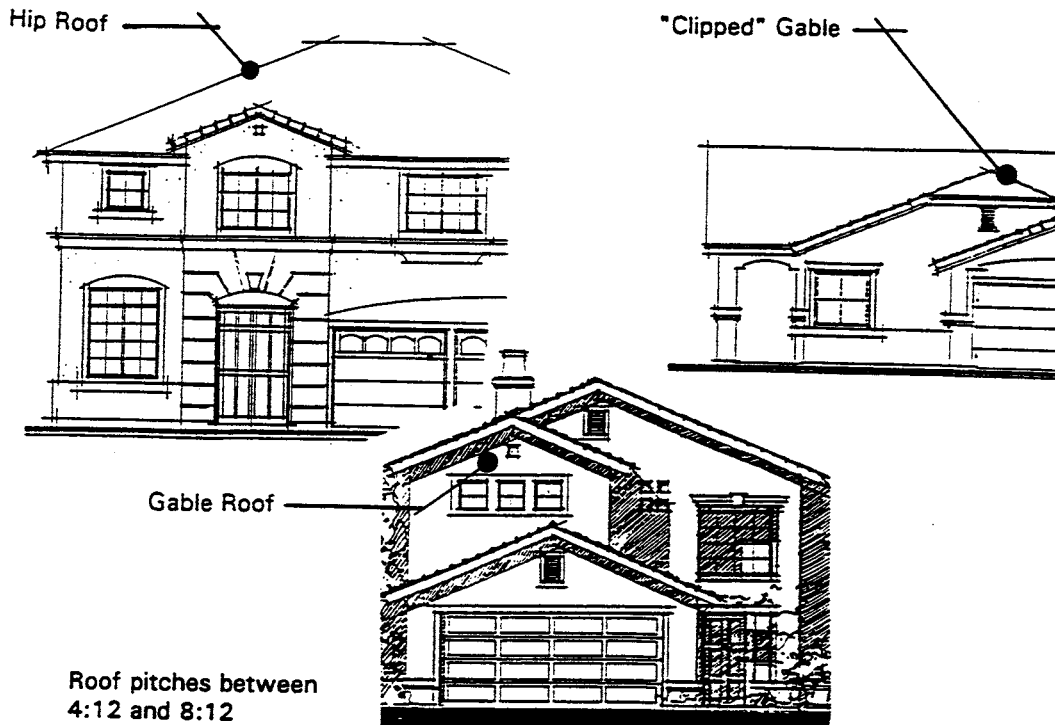
The primary roof form is a key element in creating diversity and visual interest to the street scene. In order to ensure that this diversity is achieved, the primary roof forms must meet these minimum requirements:

Side/Side Gable Min. 25% of units in subdivision

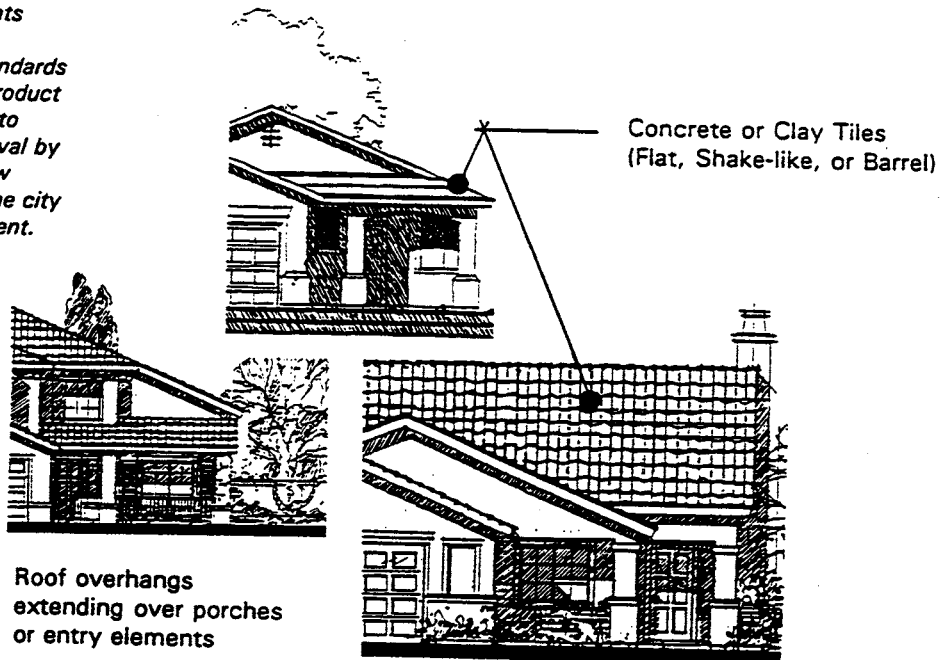
Front/Rear Gable Min. 25% of units in subdivision

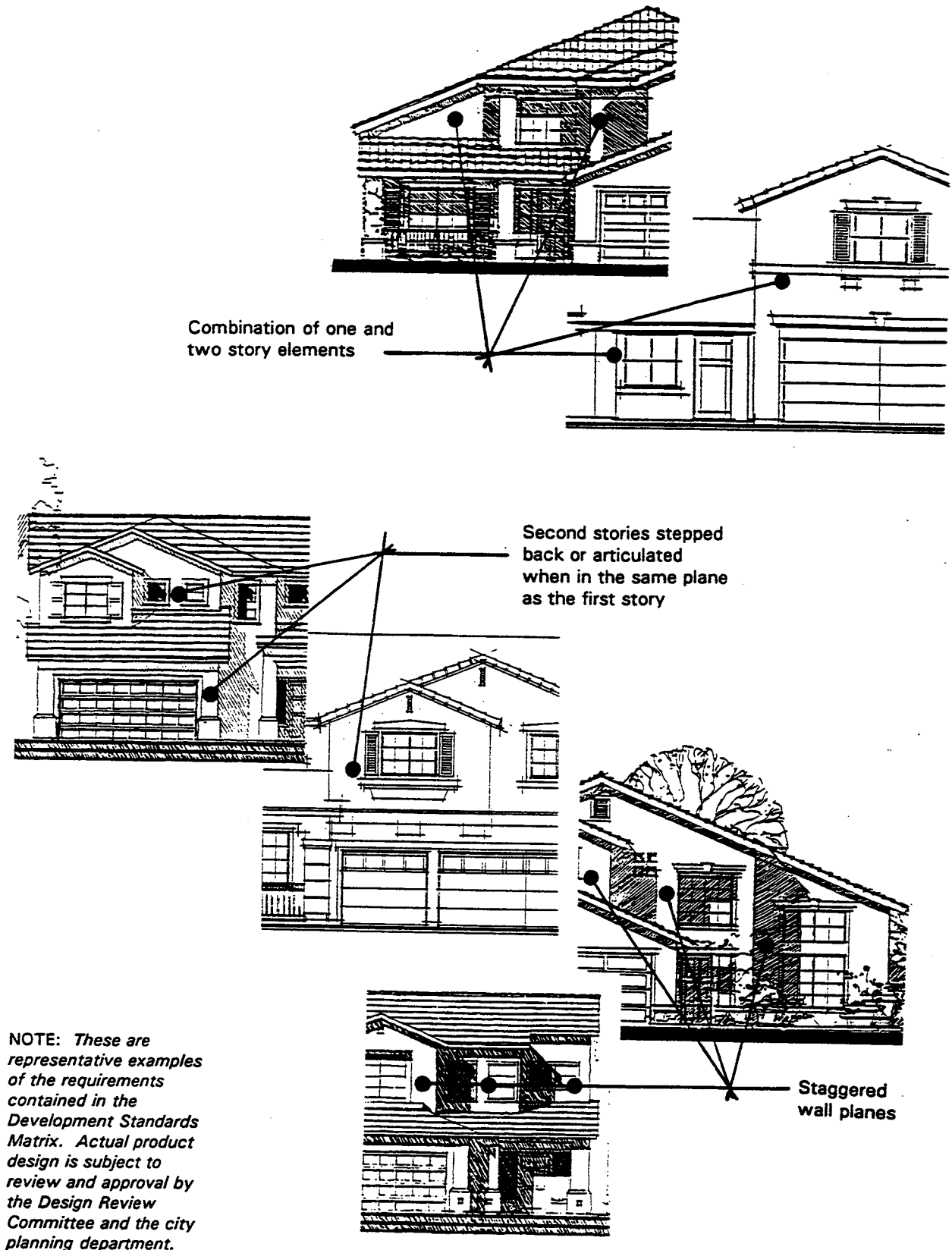
Hip Min. 25% of units in subdivision

Remaining 25% of units in subdivision
may be any of the above.

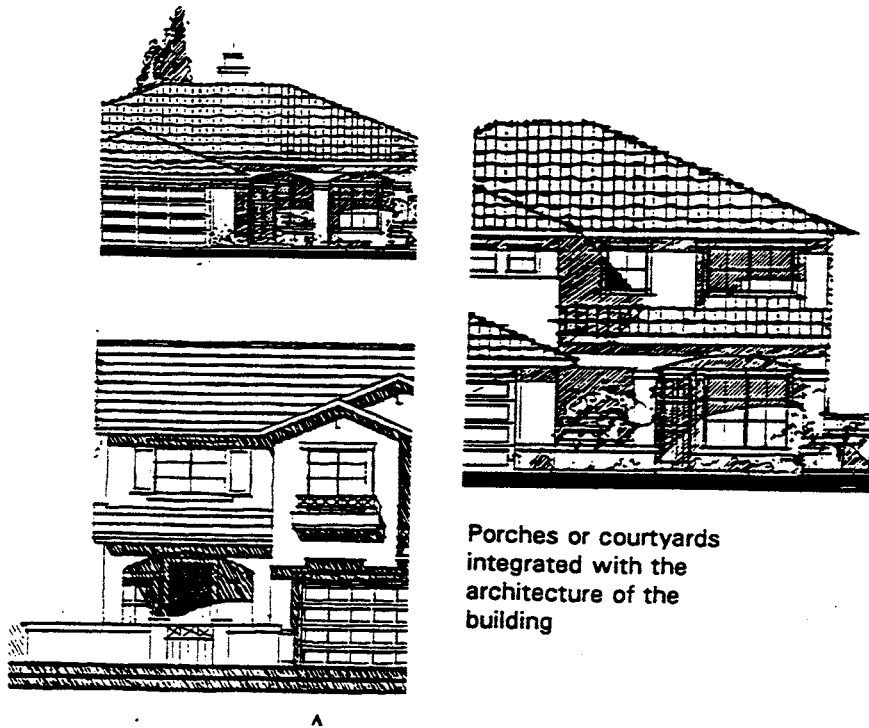


NOTE: These are representative examples of the requirements contained in the Development Standards Matrix. Actual product design is subject to review and approval by the Design Review Committee and the city planning department.

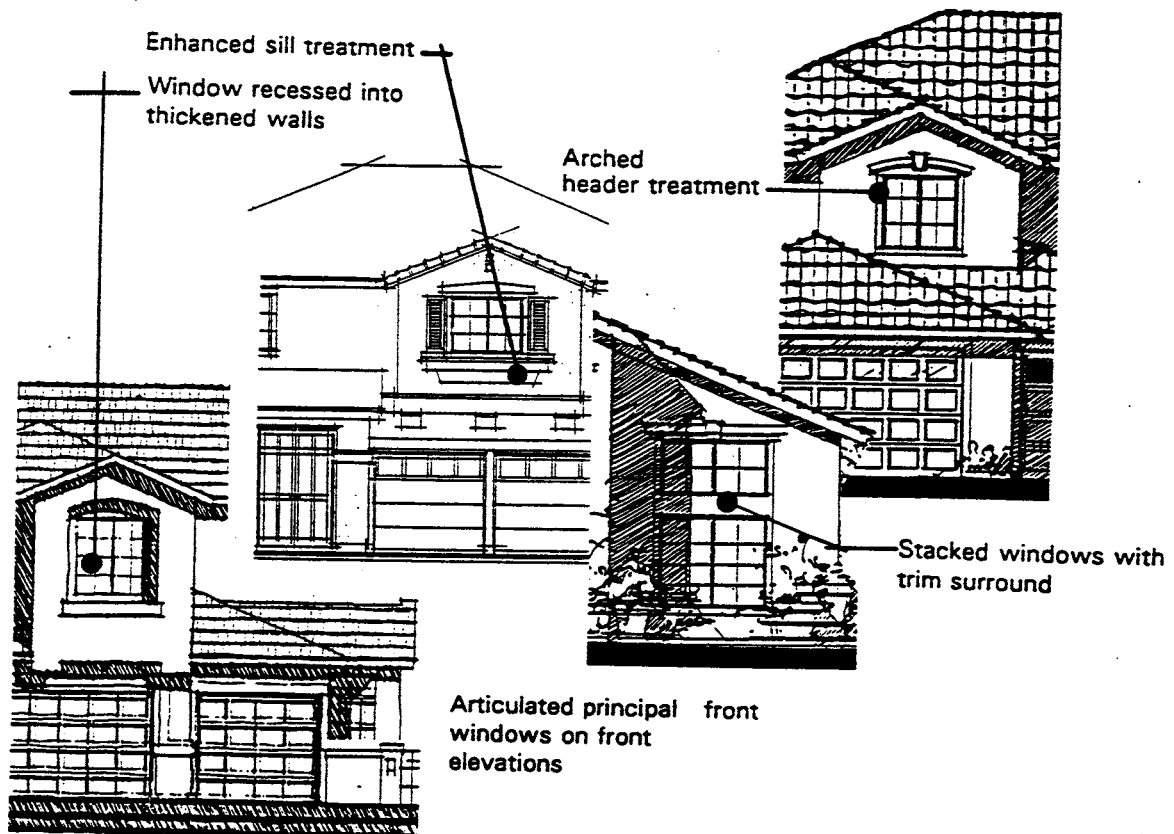


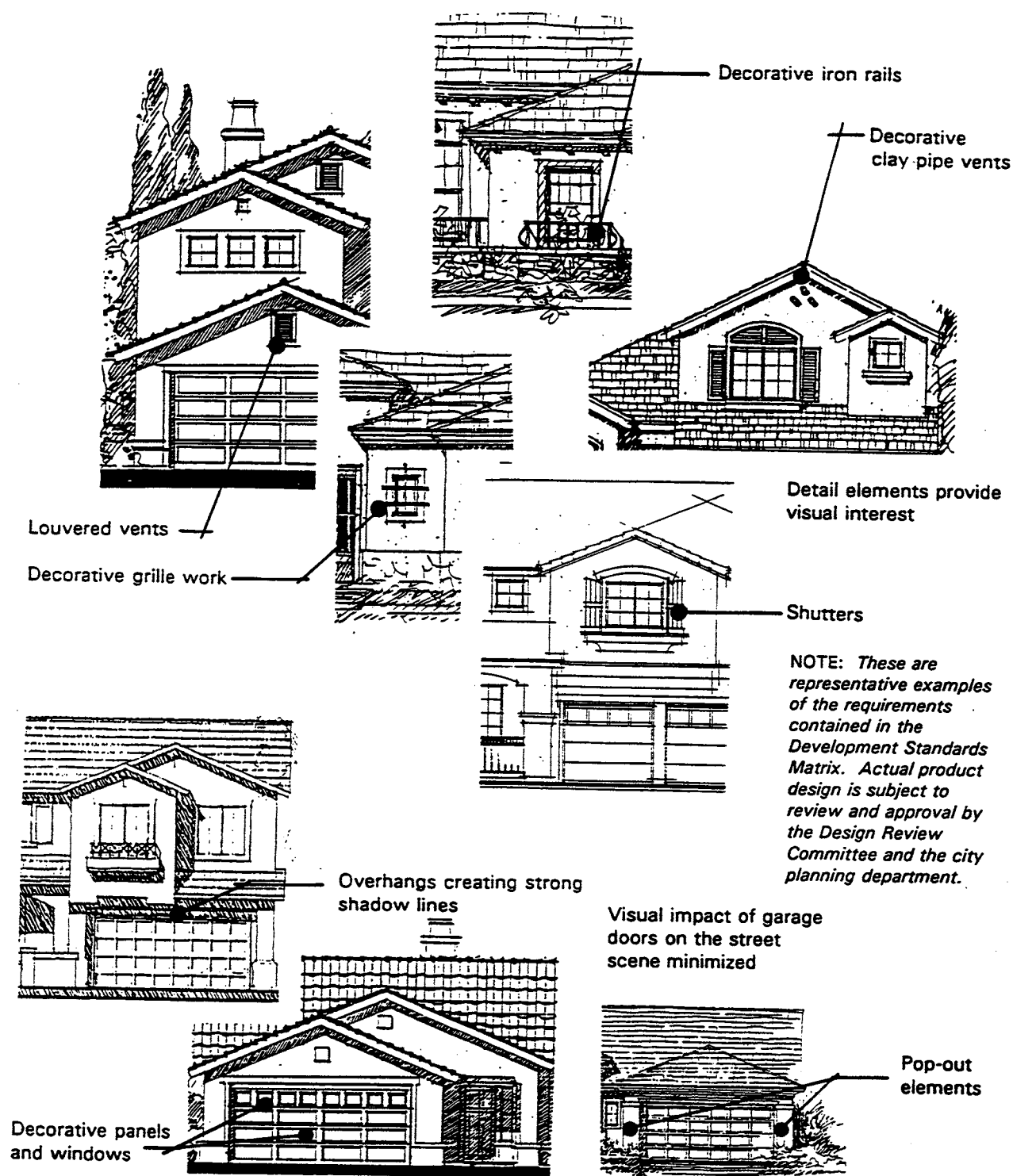


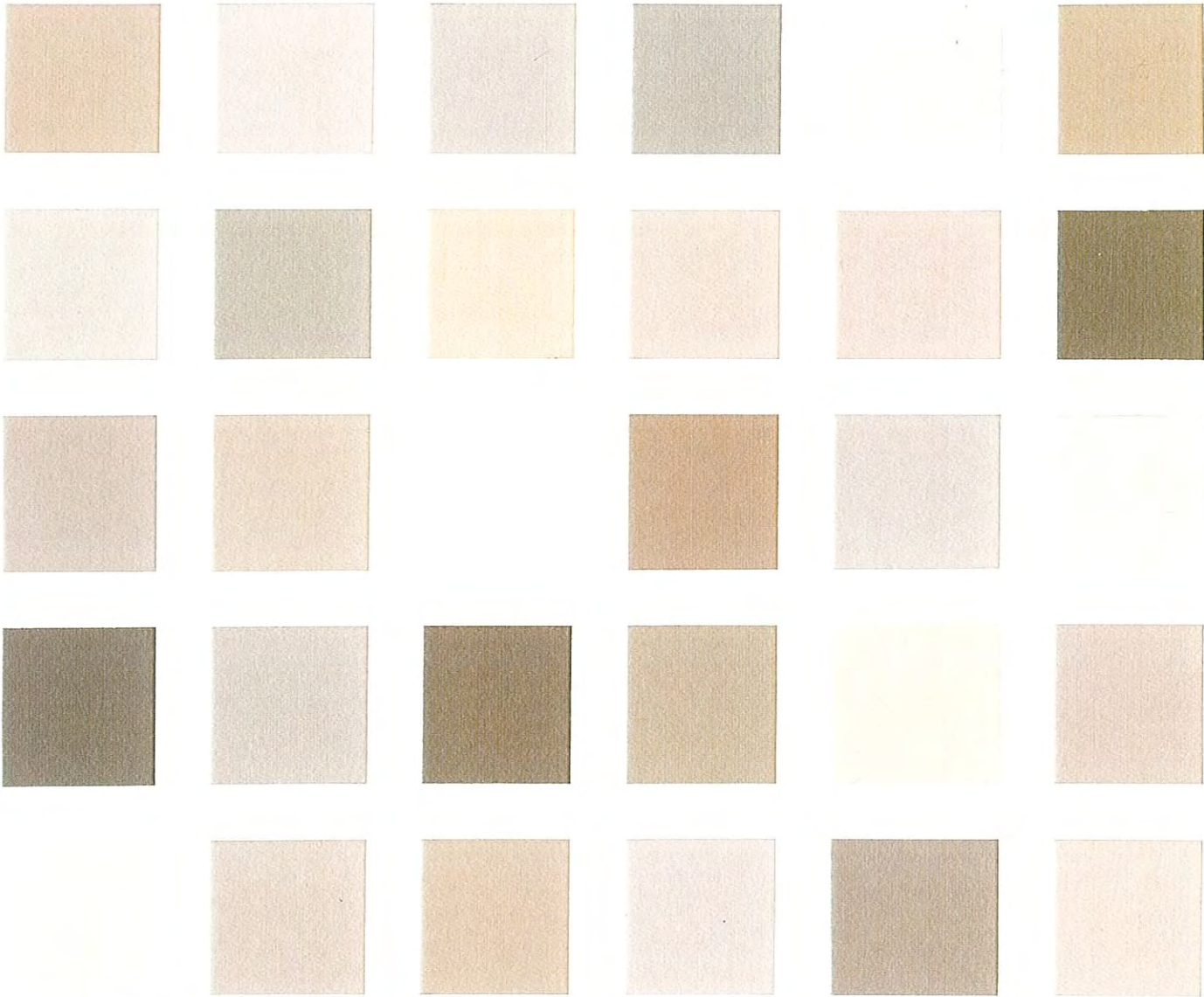
NOTE: These are representative examples of the requirements contained in the Development Standards Matrix. Actual product design is subject to review and approval by the Design Review Committee and the city planning department.



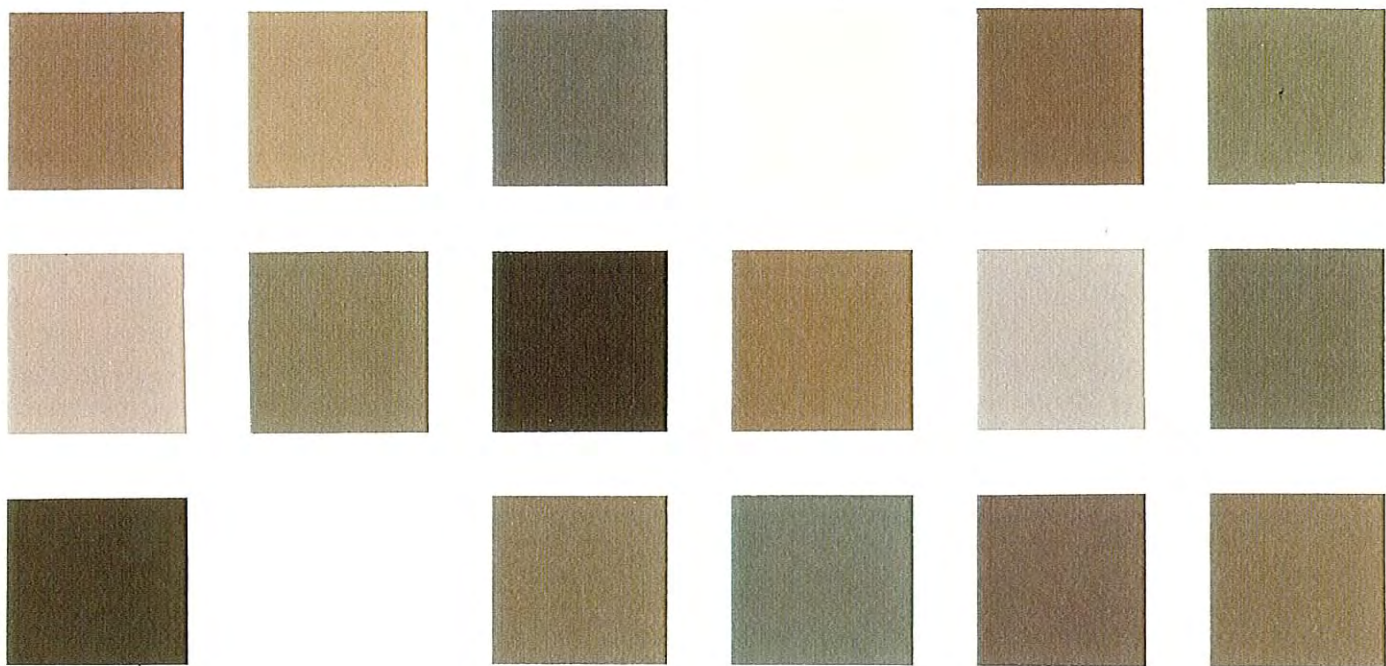
Porches or courtyards integrated with the architecture of the building



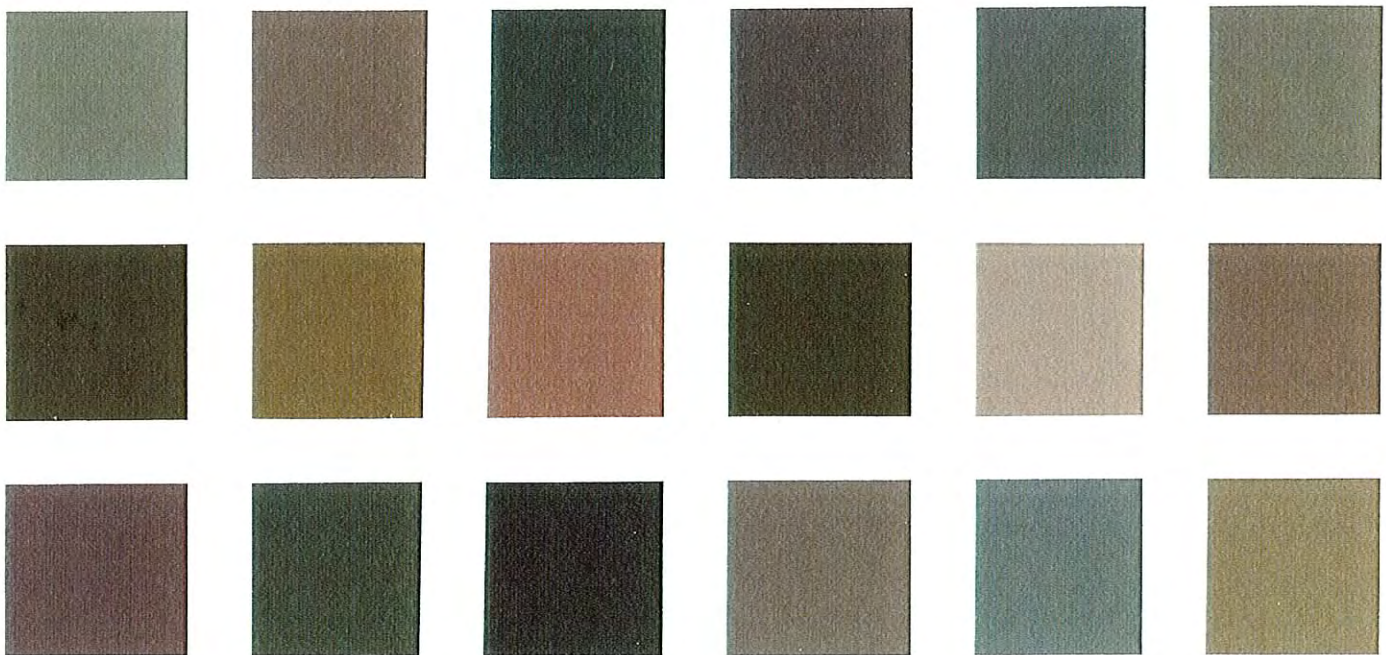




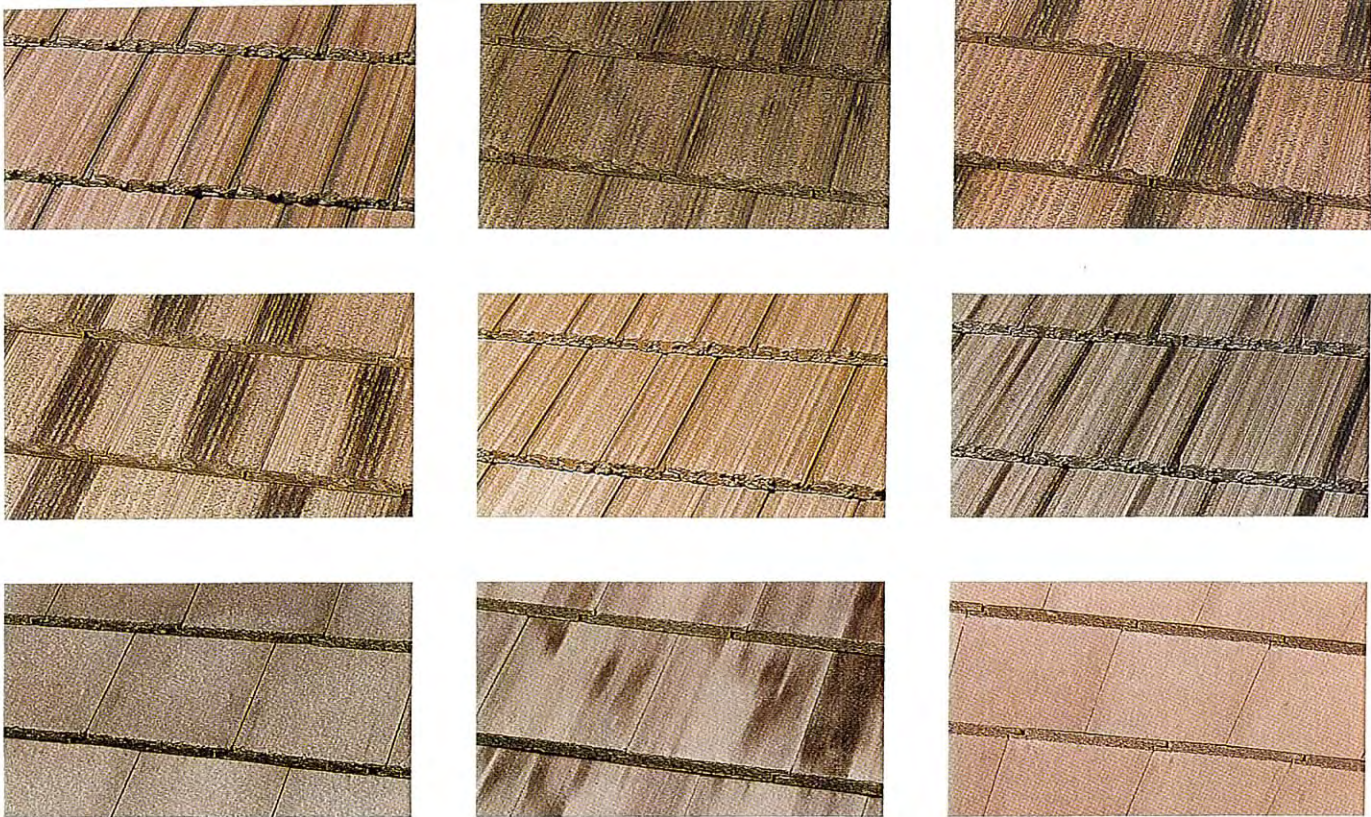
BASE COLORS



TRIM COLORS



ACCENT COLORS



FLAT CONCRETE TILES



BARREL TILES

7.4.2 Safety

Plant materials must be located to ensure safe traffic sight lines and visibility of signage and users of the trail systems. In addition, hazards to pedestrians or traffic created by plant litter, overhanging branches, thorns, sharp leaves, etc. must be minimized.

7.5 LOTS**7.5.1 General**

Landscape within the front yards shall be used to soften the building masses.

7.5.2 Corner Side Yards

Side yard landscaping is required along all collector roads with rights-of-way sixty (60) feet or wider, except for projects that have a continuous common area or trails system along said rights-of way.

7.5.3 Plant List

Plants shall conform with the Exhibit 7.12 Plant List. Unless otherwise stated, the minimum tree size is fifteen (15) gallon, the minimum shrub size is one (1) gallon. The relationship of plant height, width and caliper to the container size shall be as specified by the Arizona Nurseryman Association Standards. All plant materials shall be nursery grown, free of disease, of good habit and representing the best quality of their species.

7.5.4 Planting Size and Density

The minimum front yard requirements are as follows:

- One (1) twenty-four inch box tree/unit
- Six (6) five (5) gallon shrubs per unit
- Five (5) one (1) gallon shrubs per unit
- Turf or gravel mulch in all beds not planted with ground covers
(Turf area shall not exceed 50% of landscaped area.)

Additional corner side yard requirements where applicable, except for projects having a continuous common area or trails system along said rights-of way, are as follows:

- One (1) fifteen (15) gallon trees per unit
- Six (6) five (5) gallon shrubs per unit
- Gravel mulch in all beds not planted with ground covers

Additional front yard requirements for lots 6,000 to 9,000 square feet, where applicable, except for projects having a continuous common area or trails system along said rights-of way, are as follows:

- One (1) fifteen (15) gallon trees per unit
- Four (4) five (5) gallon shrubs per unit
- Five (5) one (1) gallon shrubs per unit

Additional front yard requirements for lots over 9,000 square feet, where applicable, except for projects having a continuous common area or trails system along said rights-of way, are as follows:

- Two (2) fifteen (15) gallon trees per unit
- Six (6) five (5) gallon shrubs per unit
- Five (5) one (1) gallon shrubs per unit

7.5.5 Gravel Mulch

Gravel mulch color shall match or blend with the gravel mulch used along the trail systems. Mulch areas must have a minimum of 75% coverage by living ground/shrub covers within three (3) years of planting.

7.5.6 Walls

Wing walls, patio walls and similar features shall be constructed of decorative masonry or shall match the building color and finish, unless otherwise approved by the Committee. Precision type standard CMU is not permitted in exposed areas.

7.5.7 Maintenance

Owners are required to maintain their landscapes in good condition at all times. This includes weekly lawn mowing, regular pruning of trees and shrubs, fertilizing,

watering, removal of dead plants, weeding, replacement of plants overseeding of lawn areas where required and regular removal of debris and trash.

7.6 TRAIL SYSTEMS

The trails systems are provided as a recreational amenity to the area residents. Refer to Exhibits 7.6 through 7.9 for conceptual layouts.

7.6.1 Planting Size and Density

The landscape areas of the Trails and streetscape areas shall have a minimum of sixty (60) percent coverage of plants at three (3) years after installation. Massed shrubbery and large ground cover beds are encouraged to meet this criterion. Turf is permitted but must not exceed thirty (30) percent of the total landscaped area. Turf shall not be installed in areas less than fourteen (14) feet wide. Minimum planting requirements for thirty (30) foot wide multi-purpose trail (number of plants per two-hundred and fifty (250) l.f.):

- Eight (8) twenty-four (24) or thirty (30) inch box trees
- Twelve (12) fifteen (15) gallon vertical evergreen trees
- Fifteen (15) five (5) gallon shrubs
- Forty (40) one (1) gallon shrubs

Minimum planting requirements for twenty (20) foot wide horse trail (number of plants per two-hundred and fifty (250) l.f.):

- Eight (8) twenty-four (24) or thirty (30) inch box trees
- Twelve (12) fifteen (15) gallon vertical evergreen trees
- Ten (10) five (5) gallon shrubs
- Thirty (30) one (1) gallon shrubs

Minimum planting requirements for twenty (20) foot wide multi-purpose trail and internal greenbelts (equestrian excluded):

- Eight (8) twenty-four (24) or thirty (30) inch box trees
- Ten (10) fifteen (15) gallon vertical evergreen trees
- Ten (10) five (5) gallon shrubs
- Twenty (20) one (1) gallon shrubs

Minimum planting requirements for 20' wide expanded trail system at Jones, Horse, Brent and Thom Streets:

- Twelve (12) twenty-four (24) or thirty (30) inch box trees
- Fifteen (15) gallon vertical evergreen trees
- Twenty (20) five (5) gallon shrubs
- Forty (40) one (1) gallon shrubs

7.6.2 Plant List

Plants shall conform with the Exhibit 7.12, Plant List. Unless otherwise stated, the minimum tree size is fifteen (15) gallon, the minimum shrub size is one (1) gallon. The relationship of plant height, width and caliper to the container size shall be as specified by the Arizona Nurseryman Association Standards. All plant materials shall be nursery grown, free of disease, of good habit and representing the best quality of their species.

7.6.3 Gravel Mulch

The following gravel mulch shall cover one hundred (100) percent of all required landscape areas except turf and ground cover beds. Gravel mulch shall be installed in a minimum two (2) inch deep layer. The use of synthetic turf or unnatural colored gravel such as white, black or green is not permitted. Boulders and rock groupings are permitted in limited areas. They should be buried a minimum of fifty (50) percent to appear as natural outcroppings. Colors of boulders shall complement the gravel mulch. Mulch areas must have a minimum of 75% coverage by living ground/shrub covers within three (3) years of planting.

Type: "Wild Rose" as supplied by Vegas Rock, or approved equal. Size 3/4".

7.6.4 Walls

Refer to Exhibit 7.4 Project theme Walls and 7.2 Entry Monumentation for additional details. Perimeter theme wall construction shall be completed prior to issuance of building permits for each construction phase.

7.6.5 Turf

All grass must be a fescue or hybrid Bermuda blend developed for the desert. Common Bermuda grass is not permitted.

Iron Mountain Ranch

7.6.6 Site Amenities

The following site amenities shall be provided for each five hundred (500) l.f. of trail system:

- One (1) Concrete bench-Precast Concrete # B1972SD-A-3-sand
- One (1) Trash Can-Precast Concrete #TR30RP-sand

7.7 COMMUNITY PARK SITE

The City of Las Vegas has designated a forty (40) acre site at the south-east corner of Bradley Road and Grand Teton Boulevard for a public park. The park connects to an area wide equestrian trail system that will benefit both the residents of Iron Mountain Ranch and the existing community. For each acre of required open space that the master plan is deficient, one acre of the city park will be developed to city standards by the Master Developer. All facilities to be constructed by the Master Developer shall be equestrian related, and shall be in substantial conformance with those previously prepared by Cella Barr Associates titled Final Master Plan 40 Acre Park (dated May 12, 1993) subject to approval by the City of Las Vegas.

The ultimate amount of city park acreage developed by the Master Developer is dependent upon the final density approved for the R-PD Master Plan. Changes to the density will result in adjustments to the city park acreage as well. The park design approval is subject to review and execution of a Parks Agreement between the City of Las Vegas and the Master Developer. Construction of Master Developer park improvements shall commence when 50% of the master plan building permits have been issued.

7.8 NEIGHBORHOOD PARK SITES

7.8.1 Purpose

Neighborhood park sites are situated throughout the neighborhoods of Iron Mountain Ranch to provide usable smaller scale recreational facilities for neighborhood residents. Each neighborhood park is a minimum of 1 acre, and a series of pedestrian linkages / greenbelts interconnect the various park sites. On average, each 1 acre neighborhood park serves up to 200 residences. The neighborhood parks total 10 acres throughout the master plan area.

7.8.2 Design Criteria

The style of the Neighborhood Park should follow the thematic element of the individual subdivision. Provisions should be made for active and passive play areas, barbecues, benches, tables, low level security lighting, shade and landscaping elements. The design will be subject to approval by the Master developer and the City of Las Vegas.

7.8.3 Requirements

All subdivisions with 10,000 square foot lots or smaller feature either of the following:

- A shared park facility between two subdivisions with a combined total of 1 acre of usable park area.
- An individual park within the subdivision, providing a minimum of 1 acre of usable park area.

All such parks must provide at least three of the permitted uses for neighborhood parks as listed in Table 5.1.2. Parking areas and restrooms shall not be counted toward this minimum requirement.

7.8.4 Maintenance

The Neighborhood Park site shall be maintained by the Homeowners Association.

7.8.5 Location

The Neighborhood Park Site shall be centrally located and accessible to all homeowners.

7.9 ENTRY MONUMENTATION**7.9.1 Purpose**

The purpose of primary, secondary and neighborhood monumentation is to give a strong and attractive statement that provides continuity throughout Iron Mountain Ranch.

7.9.2 Primary Community Entries

Primary community entry treatments are located at prominent intersections along the perimeter of Iron Mountain Ranch, providing signage monumentation, along with thematic wall and column treatments with the community logo. Please refer to Exhibit 2.1 for the location of primary community entries, and Exhibits 7.1 and 7.2 for specific design treatments.

7.9.3 Secondary Community Entries

Secondary community entries are located at other key intersections of master planned streets within Iron Mountain Ranch. These entries elements will feature the thematic wall and column treatments, including the Iron Mountain Ranch logo, as depicted in Exhibit 7.2. Please refer to Exhibit 2.1 for the location of secondary community entries.

7.9.4 Neighborhood Entries

Each neighborhood entry shall feature elements which are consistent with the primary and secondary community entries and shall include: Iron Mountain Ranch Project logo on the corner columns; accent lighting of the logo and signage; and decorative accent shrub and ground cover plantings. Neighborhood monumentation is subject to the approval by the Committee and the City of Las Vegas. Each neighborhood shall provide additional monumentation consisting of a smaller scale sign with lettering to match the primary community monument.

7.9.5 Project Name

The name of each individual project is subject to review and approval by the Committee and the City of Las Vegas. The project shall be referred to as "Project Name at Iron Mountain Ranch."

7.10 BIKE ROUTE AND TRAILS**7.10.1 Purpose**

The purpose of bike route and trails is to provide safe access for bicyclists, horses and pedestrians throughout the community.

7.10.2 Bike Routes

Streets within the development may be striped for bike routes where separate bike paths do not occur. Striping will be in accordance with the City of Las Vegas Open Space Master Plan.

7.10.3 Bike Trails

Certain Bike trails are provided throughout the development to be dedicated for bicycle and pedestrian. Refer to Exhibits 7.6 through 7.8.

7.11 GRADING AND DRAINAGE**7.11.1 General**

Owner shall minimize the visual impact of grading with retaining walls or planting. Retaining walls shall not exceed five (5) feet in height and shall be built of materials consistent with the building design. Retaining walls must be waterproofed to minimize water staining.

7.11.2 Drainage

Grading design shall ensure adequate site drainage that complies with the City of Las Vegas.

7.12 IRRIGATION**7.12.1 Irrigation Equipment**

Automatic underground irrigation systems are required for all landscape areas. Specific irrigation requirements are as follows:

- Spray head to head coverage will be required in all lawn and ground cover areas. Systems shall be designed so that peak summertime watering can be completed during the hours established by the Las Vegas Valley Water District.
- An electric, solid state controller is required and shall be equipped with a maintenance-free evapotranspiration gauge, rain shut-off device, master valve terminal and at least two (2) independent programs. Individual lots are not required to utilize the evapotranspiration gauge.

- All irrigated areas shall utilize electric valves. Manual valves will not be permitted. All valves shall be underground and installed in valve boxes. Proper backflow prevention is required.
- Drip irrigation shall be utilized for all plant materials one (1) gallon or larger. Appropriate filtration and pressure regulation is required. Trees, shrubs and ground covers shall be provided with separate valves. Low growing ground covers may receive pop-up spray overhead irrigation. Fixed riser type sprinklers are not permitted.
- Paved surfaces eight (8) feet or wider, including driveways within Street Landscape Areas, shall be sleeved for pressure supply lines, non-pressure piping and control wires.
- Spray heads shall not throw water onto parking areas, streets, signs, walls, fences, buildings, structures or sidewalks.

7.13 SIGNAGE

The master developer shall develop a master signage program for Iron Mountain Ranch to ensure consistency with the overall community image. The master signage program shall be subject to review and approval by city staff, and will address the following:

- Entry Signage
- Temporary Directional Signage
- Future Facility Signage
- Trail Signage
- Temporary Construction Signage
- Builder / Developer Model Complex Identification Signage

7.14 STREET LIGHTING

The design intent for lighting is to provide safe and functional lighting in an aesthetically pleasing, visually unobtrusive manner. All lighting plans, whether for safety or aesthetics, must be submitted for approval by City of Las Vegas Planning and Development. Lighting shall be accordance with the following criteria:

7.3 GENERAL LANDSCAPE CRITERIA

7.3.1 Purpose

The following landscape architecture criterion provides specific requirements for the single family developments. The objective of this section is to establish a pleasant and attractive landscape framework for development. This framework will help provide design continuity and establish an identifiable visual character that enhances a community image and the value of the development.

7.3.2 Landscape Concept

The overall landscape concept for the projects is an attractive, diverse palette of water conserving plants with limited use of lawn. The plant groupings and specific plants for the various trail systems and landscape areas are described in Section 4.4 and Exhibits 7.6 thru 7.10.

7.3.3 Minimum Landscape Areas

A thirty (30) foot wide multi-purpose trail system is required along the east side of Bradley Road south of Grand Teton Drive. Refer to Exhibit 7.6. A twenty-five (25) foot wide horse trail is required along north side of Grand Teton Drive and the east side of Bradley north of Grand Teton Drive. Refer to Exhibit 7.7. A twenty (20) foot wide multi-purpose trail (equestrian excluded) is required along the south side of Grand Teton Drive and the west side of Bradley Road north of Grand Teton Drive. Refer to Exhibit 7.8. A special additional five (5) foot wide landscape area occurs on the Private Drives located on Jones Boulevard and Bradley Road. Refer to Exhibit 7.9. Additionally, 20' wide greenbelt corridors will be required as internal linkage trails throughout the site with a 5' wide meandering trail and appropriate landscaping. Refer to Exhibit 7.10.

7.4 PLANTING DESIGN

7.4.1 Design Intent

Plants should be arranged to highlight building entries, soften building and wall masses and provide scale to site development. Landscape materials shall be in accordance with Exhibit 7.12 Plant List.

- Street lighting and installation shall conform to the City of Las Vegas standards.
- Area lighting shall be provided along all public and private streets. Light standards and pole heights shall be scaled to the street dimension illumination requirements as per code.
- Hidden source lighting is required. Lamp sources shall not be visible or obtrusive into any neighboring area as per code.
- Pedestrian areas, including off-street trails, pathways, parks and other public areas, shall be illuminated during the hours of darkness.
- Task lighting shall be installed to emphasize major project entry signage and selected landscape features.

SCREENING SHRUBS AGAINST WALLS
THEME WALL W/PROJECT LOGO ON ALL COLUMNS
SIDES THAT ARE VISIBLE FROM STREET

VERTICAL ACCENT TREE

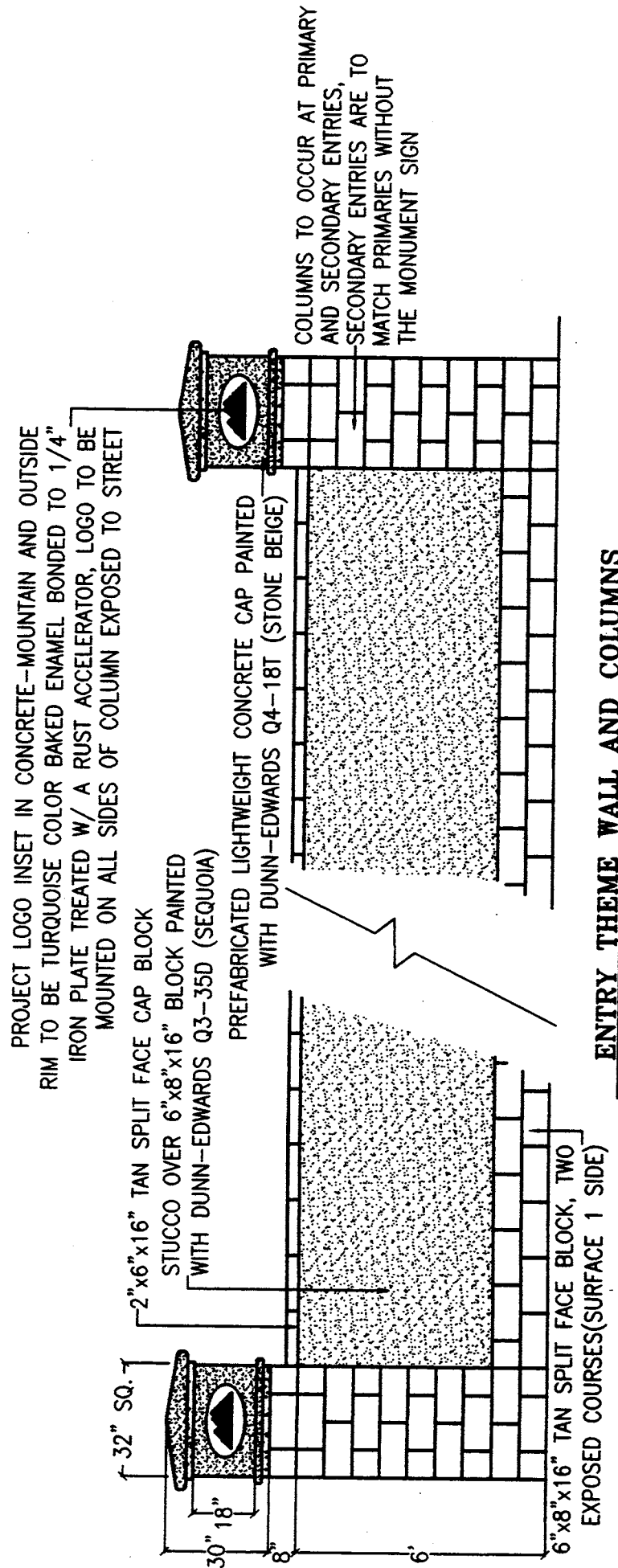
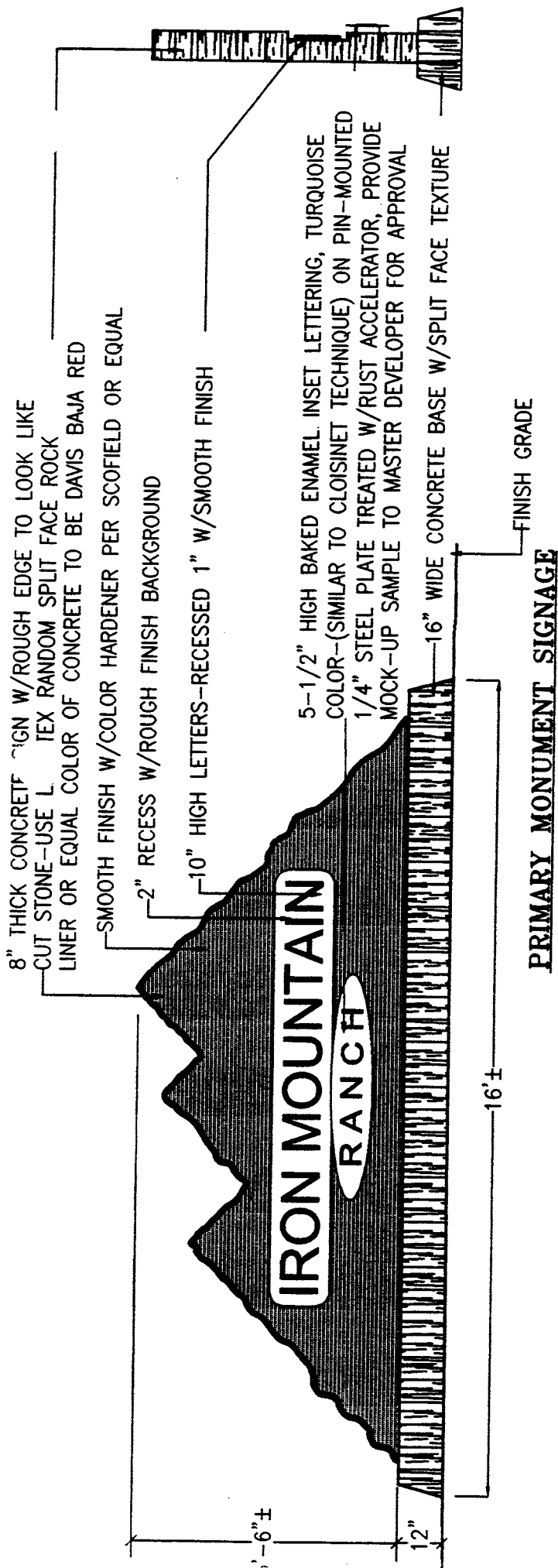
FLOWERING ACCENT TREE

ACCENT SHRUBS

LOW-GROWING FLOWERING SHRUBS

MONUMENT SIGN PER EXHIBIT 7.2

IRON MOUNTAIN
RANCH



**ENTRY THEME WALL AND COLUMNS
FOR PRIMARY AND SECONDARY ENTRIES**

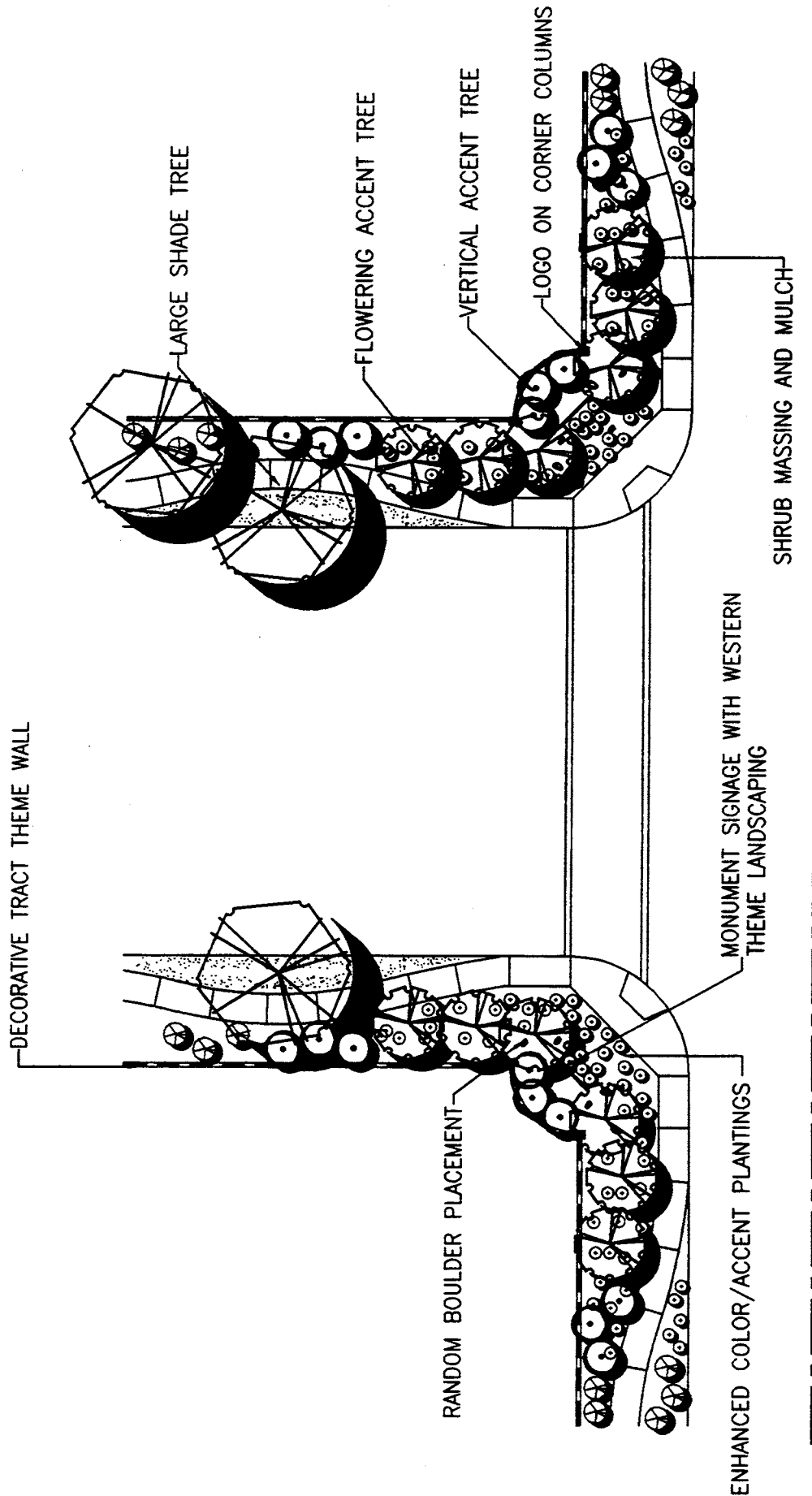


EXHIBIT 7.3
 PRIMARY ENTRY TREATMENT

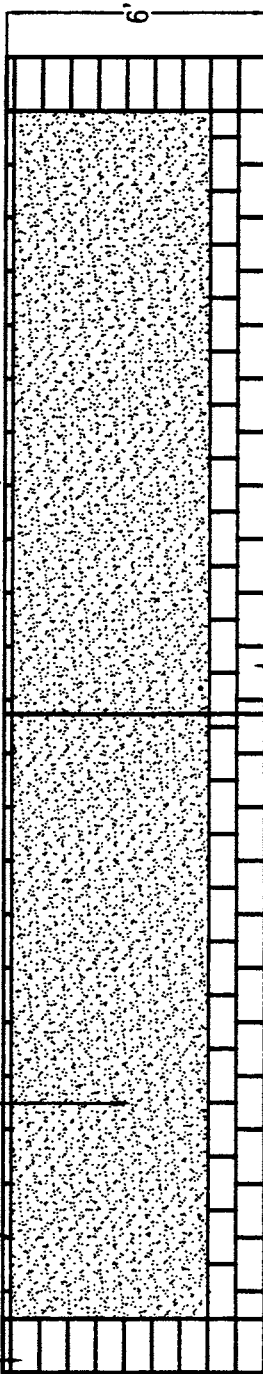
NOTE: WHEN WALL ABUTS CUL-D-SAC, TUBULAR STEEL FENCING SHALL BE INSTALLED IN 24' WIDE x 5' HIGH SECTIONS. FENCING IS TO HAVE 2" SQ. STRUCTURAL STEEL POSTS AT 8' O.C. WITH 1" TOP & BOTTOM RAILS AND 3/4" PICKETS @ 4" O.C., POWDER COAT ALL STEEL TAN TO MATCH SPLIT FACE BLOCK

8x8x16 TAN SPLIT FACE BLOCK COLUMN W/2"x8" CAP STONE-INSTALL COLUMNS AT PROPERTY LINE INTERSECTIONS OF WALL (MAX. 60' O.C.)

2x6x16 TAN COLOR SPLIT FACE CAP BLOCK

STUCCO OVER 6x8x16 TAN CMU BLOCK

PAINTED W/DUNN-EDWARDS Q3-35D (SEQUOIA)



PROVIDE COLD JOINTS AT 20' O.C. ±
DIVIDE WALL SECTIONS EVENLY

PROVIDE TWO EXPOSED COURSES OF 6x8x16 TAN SPLIT FACE BLOCK AT BOTTOM OF WALL, ALL SPLIT FACE BLOCK TO BE TEXTURED ONE SIDE

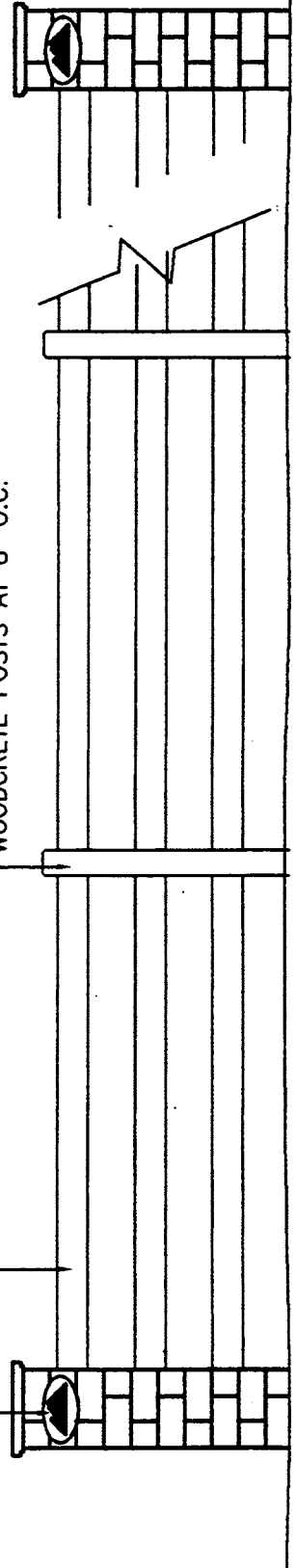
ELEVATION OF THEME WALL

N.T.S.

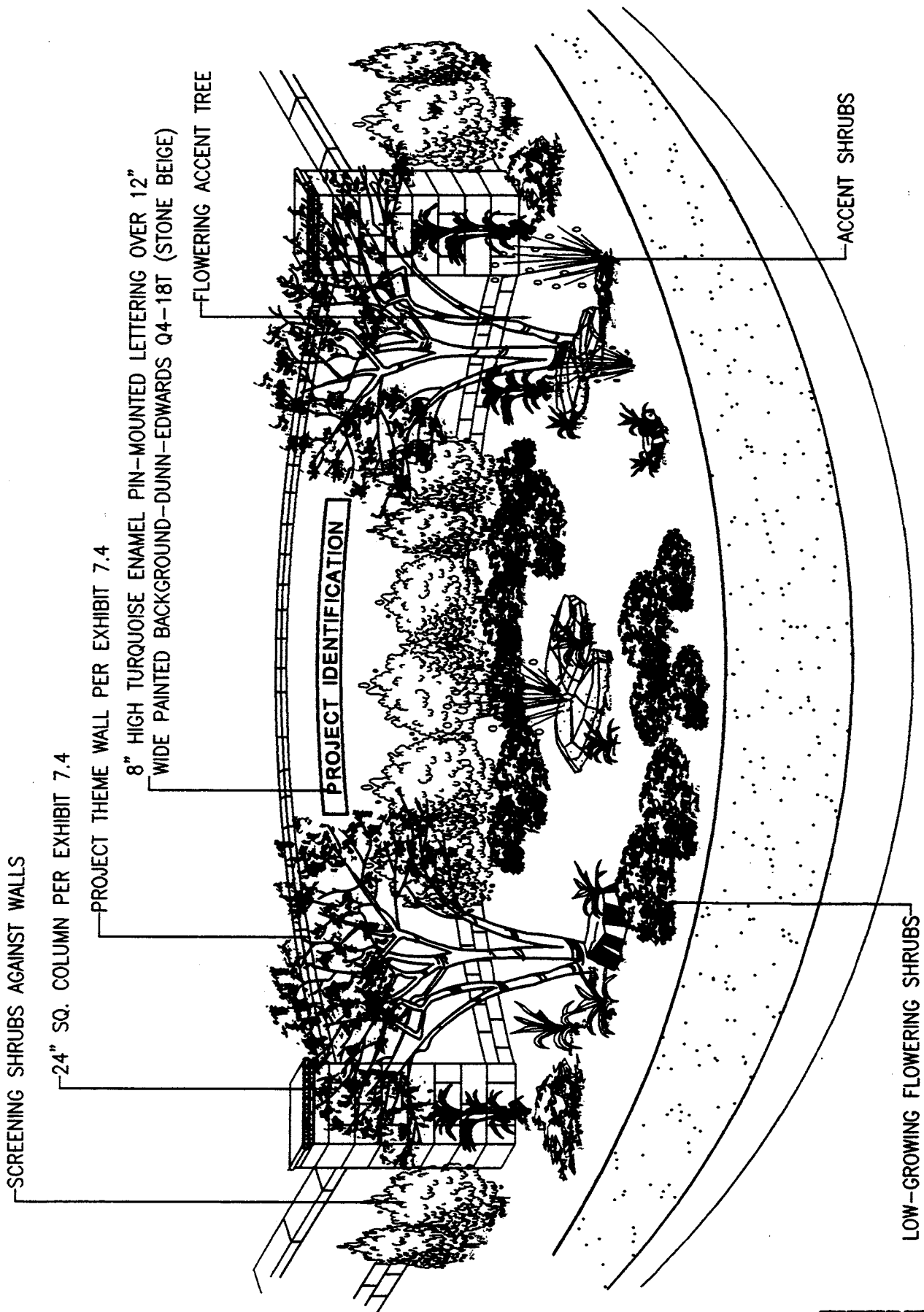
18" SQ. SPLIT FACE COLUMNS AT 50'± O.C
WITH PROJECT LOGO PER EXHIBIT 7.2

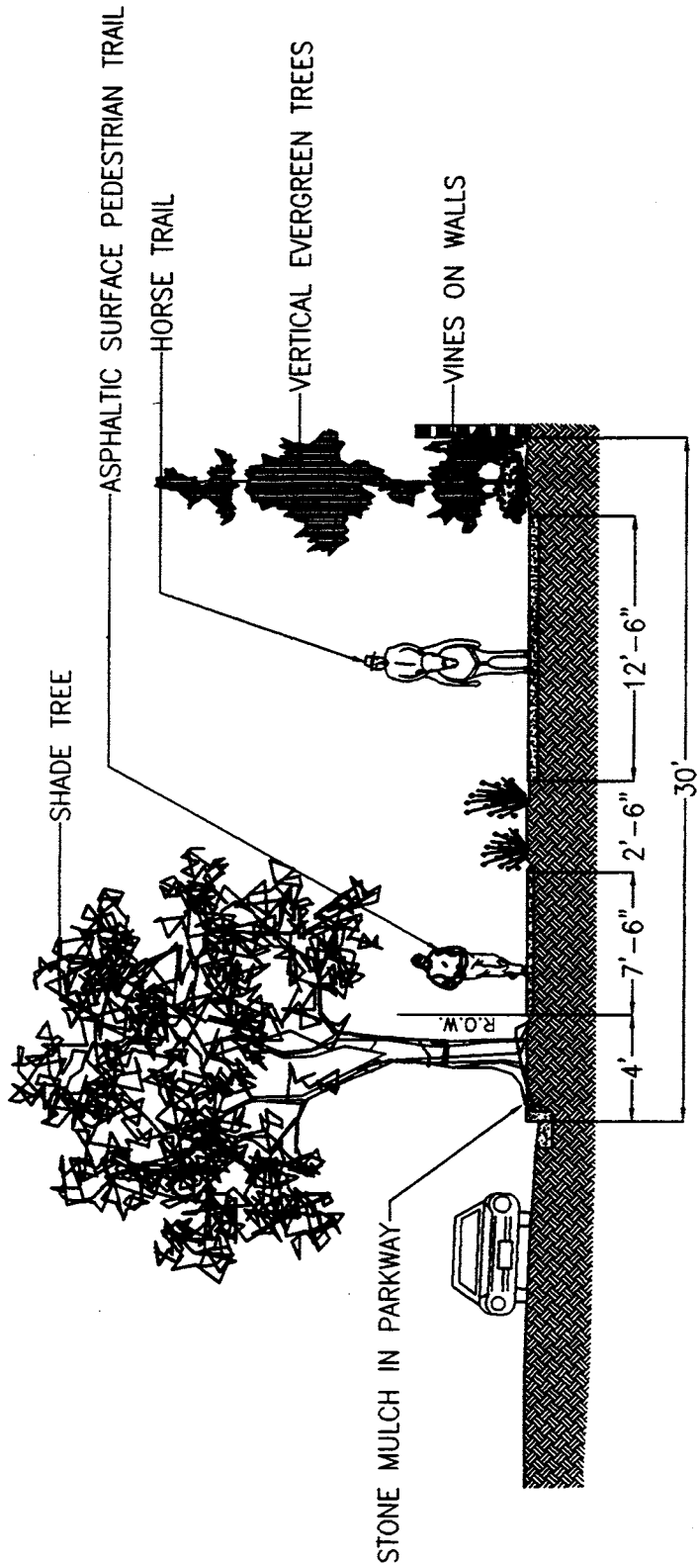
WOODCRETE CONCRETE SPLIT RAIL VENCING-TAN COLOR

WOODCRETE POSTS AT 8' O.C.

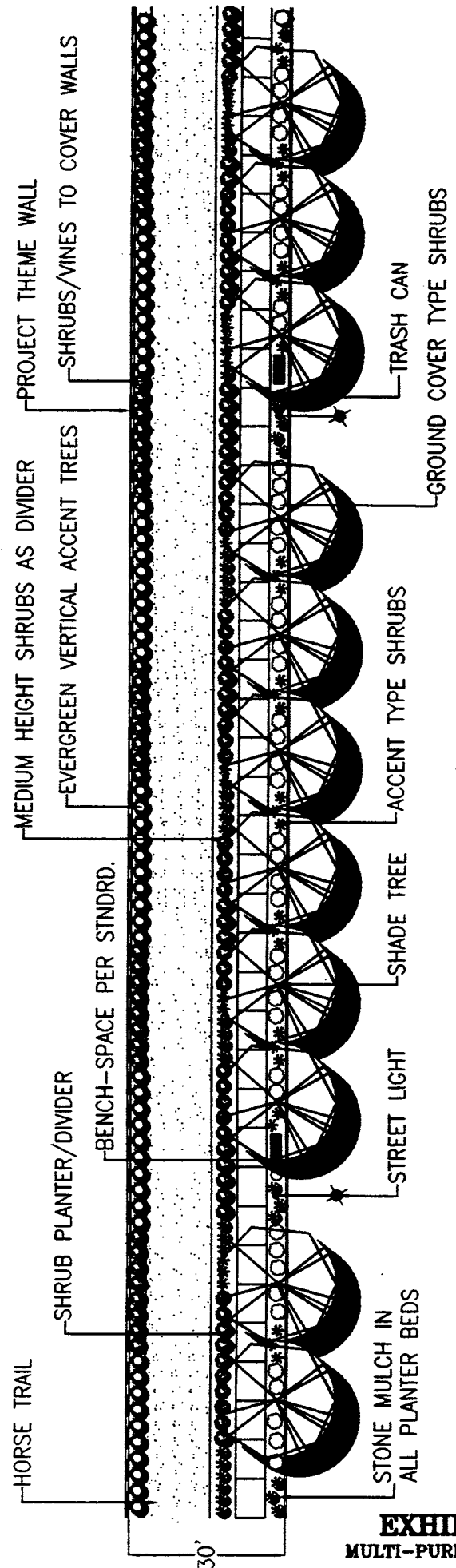


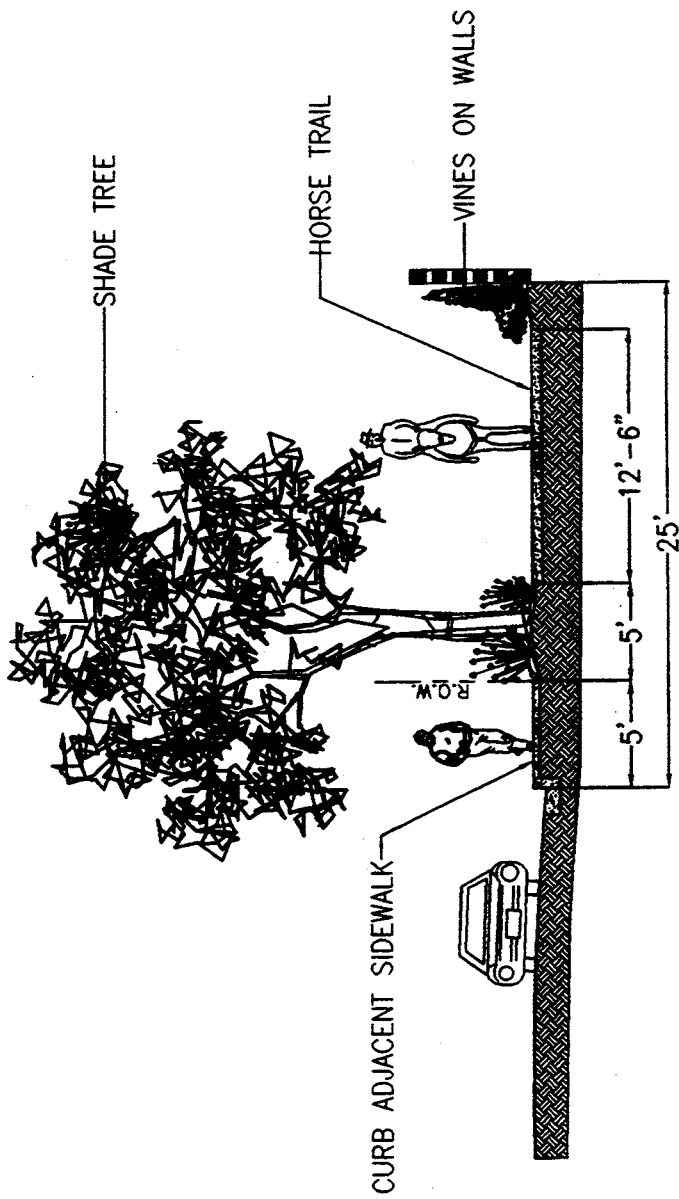
ELEVATION OF SPLIT RAIL FENCE



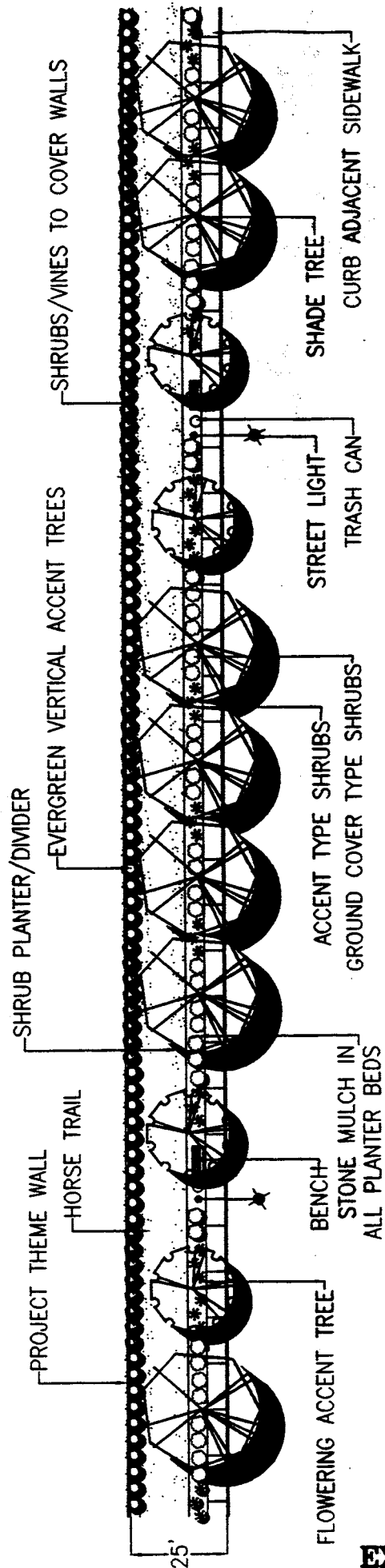


SECTION A-A

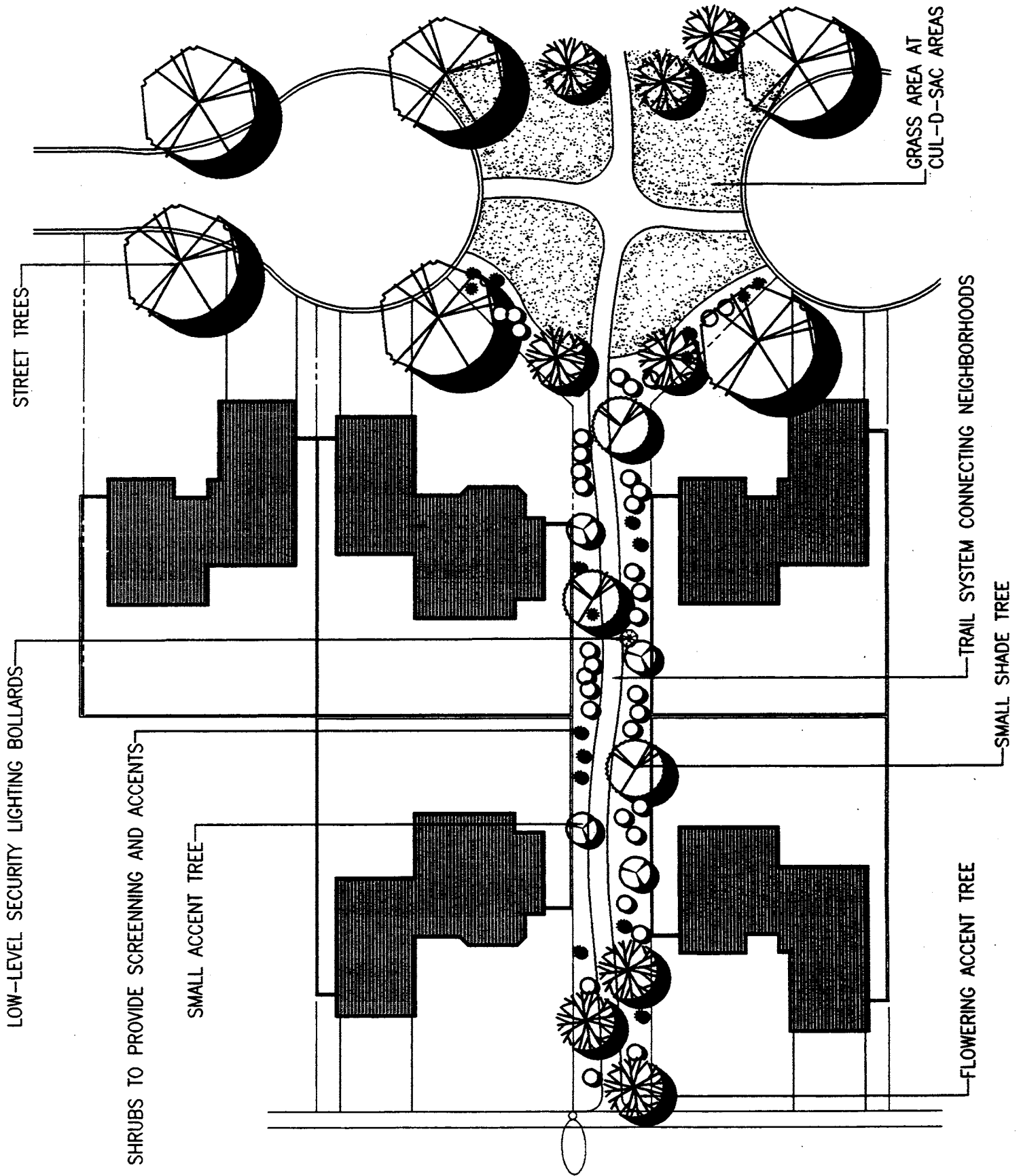




SECTION B-B







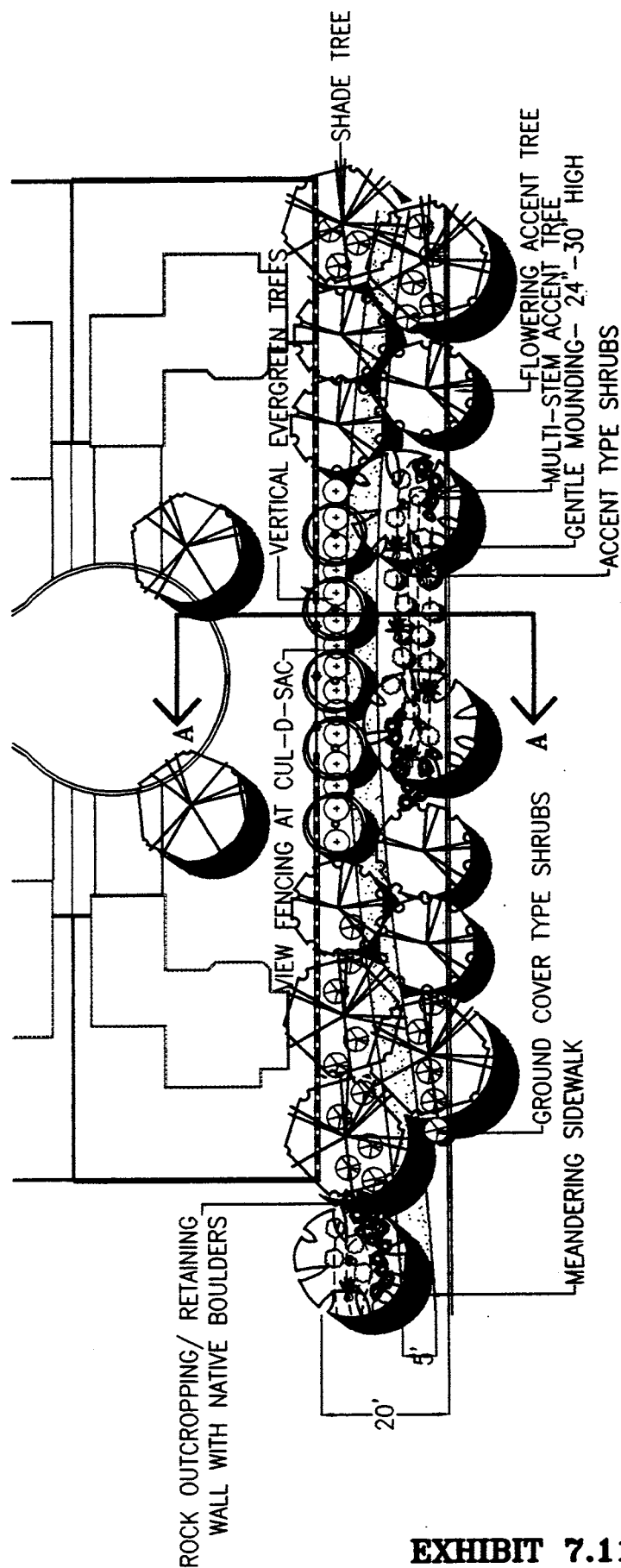
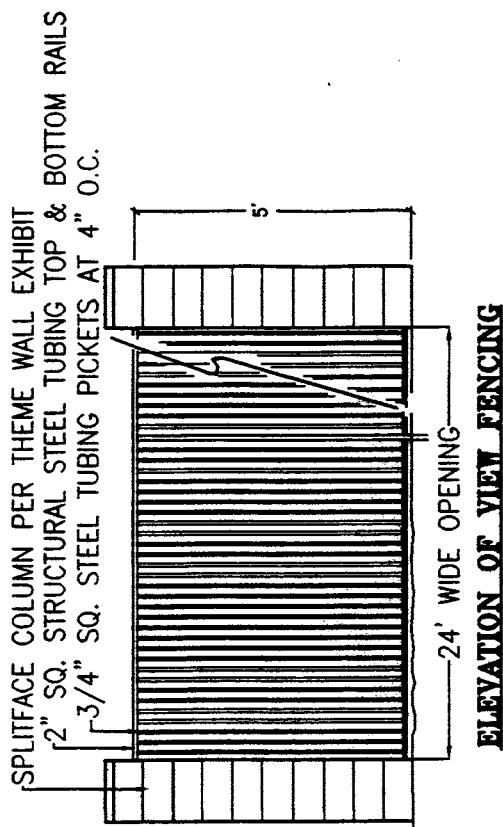
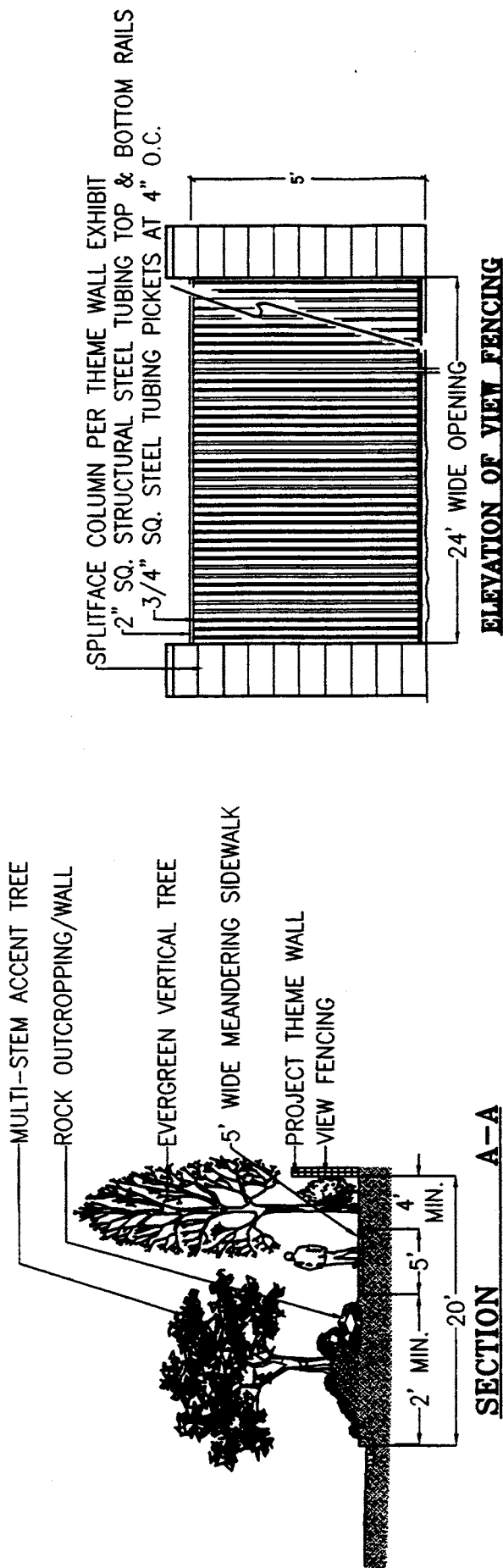


EXHIBIT 7.11
 EXPANDED TRAILS ON JONES,
 HORSE, BRENT & THOM STREETS

BOTANICAL NAME**COMMON NAME****SIZE****TREES**

Acacia smallii	Sweet Acacia	24" Box
Albizia j. 'Rosea'	Silk Tree	30" Box
Cupressus arizonica	Arizona Cypress	15 Gallon
Eucalyptus microtheca	Coolibah	15 Gallon
Fraxinus o. 'Raywood'	Raywood Ash	24" Box
Gleditsia t. 'Shademaster'	Shademaster Locust	24" Box
Lagerstroemia indica	Crape Myrtle	24" Box
Pinus eldarica	Mondel Pine	24" Box
Pinus halepensis	Aleppo Pine	24" Box
Pistacia chinensis	Chinese Pistache	24" Box
Pittosporum phylliraeoides	Willow Pittosporum	24" Box
Prosopis c. 'Thornless'	Thornless Mesquite	24" Box
Prunus c. 'Krauter Vesuvius'	Purple leaf Plum	24" Box
Pyrus c. 'Bradfordi'	Bradford Pear	24" Box
Quercus ilex	Holly Oak	24" Box

SHRUBS

Acacia redolens	Prostrate Acacia	1 Gallon
Annisodonteia hypomandrum	Pink Hibiscus	5 Gallon
Abelia g. 'Edward Goucher'	Glossy Abelia	5 Gallon
Calliandra eriophylla	Fairy Duster	5 Gallon
Cotoneaster dammeri	Bearberry	1 Gallon
Eleagnus ebbingei	Ebbing's Silverberry	5 Gallon
Euryops p. 'Viridis'	Green Euryops	5 Gallon
Hesperaloe parviflora	Red Yucca	1 Gallon
Juniperus species	Juniper	5 & 1 Gallon
Lantana montevidensis	Trailing lantana	1 Gallon
Lantana radiation	Bush lantana	5 Gallon
Leucophyllum f. 'Green Cloud'	Green Texas Ranger	5 Gallon
Muhlenbergia parviflora	Deer Grass	1 Gallon
Nandina domestica	Heavenly Bamboo	5 Gallon
Pittosporum tobira	Mock Orange	5 Gallon
Yucca Whipplei	Whipple Yucca	5 Gallon
Caesalpinia gilliesii	Desert Bird of Paradise	5 Gallon

GROUND COVERS

Baccharis 'Centennial'	Coyote Bush	1 Gallon/Flat
Myoporum p. 'Putah Creek'	Putah Creek Myoporum	1 Gallon/Flat
Rosmarinus o. 'Prostrata'	Dwarf Rosemary	1 Gallon/Flat

EXHIBIT 7.12 PLANT MATERIALS LIST

7.15 PARKING STANDARDS

7.15.1 Required Parking Spaces: Required parking spaces shall be in accordance with City of Las Vegas regulations.

7.15.2 Covered Parking Spaces: A minimum two-car garage is required on each lot.

7.16 SITE DESIGN DEVELOPMENT STANDARDS

	4,500 S.F. LOTS	7,000 S.F. LOTS	10,000 S.F. LOTS	20,000 S.F. LOTS ⁽¹⁾	30,000 S.F. LOTS ⁽¹⁾
MINIMUM SETBACKS					
Front					
Living Area/Porch/Side Entry	14'	14'	16'	20'	35'
Garage	18'	18'	20'	25'	35'
Side ⁽²⁾	5'	5'	10'	10'	10'
Corner Side	10'	10'	15'	15'	15'
Rear ⁽³⁾	15'	15'	20'	35'	35'
MAXIMUM HEIGHT	35'	35'	35'	35' ⁽⁴⁾	35'
MAXIMUM LOT COVERAGE	50%	50%	50%	50%	50%
HORSE CORRAL OR STABLE	--	--	--	<p>Stables or corrals shall be placed at least 50' from any adjacent dwelling on an adjacent residential lot and at least 100' from the front property line. Front setback may be reduced to 75' if corral or stable is fully screened from the street.</p> <p>Structures shall be placed at least 5' from any side or rear property line abutting a residential zoning district.</p> <p>Barns and other structures shall conform to the standards for accessory buildings.</p>	

ACCESSORY STRUCTURES

Accessory Only to Main Use. No accessory structure shall be erected or moved onto any lot prior to construction of the main building, unless a building permit has been issued for the construction of the main building.

Height. A detached accessory structure shall not exceed two stories in height (maximum 35'), or the height of the main building, whichever is less.

Size and Coverage. A single, detached accessory structure shall not exceed the floor area of the principal dwelling unit construction on the same lot. In addition, the main aggregate total of the ground floor areas of all accessory building shall not cover more than 50% of the rear yard; and further, the aggregate total of the ground floor areas of all structures and swellings shall not exceed the percentage of lot coverage permitted by the underlying development standards.

Side and Rear Yards. Detached accessory structures (excluding patio covers) in the rear yard area must be located a minimum distance of 3' from the side and rear property lines and must be separated a minimum distance of 6' from the main dwelling. On corner lots, the roof of an accessory structure may be attached to the main dwelling, if there is a minimum 6' separation between the walls of the accessory structure and the main building and provided that at least two sides of the breezeway are open. A gate or fence which is at least 50% open construction may be attached to one end of the breezeway. In addition, accessory structures in the rear yard of corner lots may not be located closer to the side property line than that required for the main dwelling.

NOTES:

1. All minimum 20,000 S.F. and 30,000 S.F. lots shall have a minimum frontage width of 100 lineal feet.
2. A minimum 10' side setback is required for the following properties:
 - Side yards immediately adjacent to the south side of Grand Teton Dr. between Jones Blvd. and Bradley Rd.
 - Side yards immediately adjacent to the north side of Horse Dr. between Jones Blvd and Bradley Rd.
3. 20,000 S.F. lots fronting onto Cul-de-Sacs or Knuckles are allowed 30' rear yard setbacks.
4. The four lots along the north boundary of Planning Area 10 are limited to single story houses with a maximum height of 25'.

7.17 PROJECTIONS

7.17.1 PATIO COVERS

A patio cover may encroach to within 5' from the rear and side property lines in parcels containing up to 10,000 S.F. lot programs; may encroach 10' from the rear property line in parcels containing 20,000 S.F. lots; and may encroach 15' from the rear property line in parcels containing 30,000 S.F. lots; provided the property owner secures the signatures of all immediately adjacent property owners verifying that they are not opposed to the encroachment.

- The setback shall be measured from the supporting posts, however, the overhang of a patio cover may not extend closer than 3' from a property line.
- Any patio cover extending into the established setback area for the lot or parcel may not be enclosed with any materials, including wood, metal, canvas, plastic, glass or any other screening material. An enclosed patio cover must conform to the setback standards applicable to the main dwelling.
- The height of the patio cover shall not exceed 12'.

7.17.2 ARCHITECTURAL FEATURES

Bay windows, fireplaces, roof eaves, cabinets designed to screen utility meters and similar architectural features may encroach no more than 2' into any required setback area, provided that the encroachments:

- Remain at least 3' from the property lines; and
- Do not increase the living spaces of the structure at the floor level.

7.17.3 LOTS ADJACENT TO OPEN SPACE

On any lot which adjoins a park area, common open space or similar open space, open balconies may extend up to 5' into the required setback for the dwelling and toward the open space; provided, however, that the projection extends no closer than 3' from the property line.

7.17.4 MECHANICAL EQUIPMENT

Mechanical equipment such as air-conditioning units, pool filtering and heating equipment, water softeners, and similar mechanical equipment may occupy the required rear and side yard setback areas if totally screened from abutting lots and streets by fences, walls, or landscaping, and if such mechanical equipment does not restrict required access through such setback areas as determined by the Department of Planning and Development.

CHAPTER 8

IMPLEMENTATION

8.1 INTRODUCTION

The Iron Mountain Ranch Residential Planned Development Master Plan (R-PD Master Plan) will be adopted by the City of Las Vegas City Council in accordance with the City of Las Vegas Zoning Code. Following adoption by the City, the R-PD Master Plan will serve as the design and development standards for the Iron Mountain Ranch Planned Development Zone. The R-PD Master Plan addresses general provisions, permitted uses, development standards, and community design guidelines consistent with the requirements of the City of Las Vegas Zoning Code.

8.2 IMPLEMENTATION

Development within the Iron Mountain Ranch Planned Development shall be implemented through the review and approval by the City of tentative tract maps and site development plans. The implementation process described below provides for the mechanisms for review and approval of development projects within Iron Mountain Ranch consistent with the R-PD Master Plan goals and objectives.

8.2.1 Applicability

All development proposals within Iron Mountain Ranch shall be subject to the implementation procedures established herein. Whenever the provisions and development standards contained herein conflict with those contained in the City of Las Vegas Zoning Code, the provisions of the R-PD Master Plan shall take precedence.

8.2.2 Interpretation

Unless otherwise provided, any ambiguity concerning the content or application of the Iron Mountain Ranch R-PD Master Plan shall be resolved by the City of Las Vegas Director of Planning and Development in a manner consistent with the goals, policies, purpose, and intent established in the R-PD Master Plan.

8.2.3 Development Review Process

Subdivision Maps

Tentative Tract Maps will be reviewed and approved pursuant to applicable subdivision laws of the State of Nevada and the City of Las Vegas Subdivision Ordinance. Tentative Tract Maps shall be consistent with the applicable provisions of the R-PD Master Plan with respect to land use, site planning criteria, and infrastructure requirements and phasing.

Site Development Plan Review

Site Development Plan approval is required for all development proposals within the Iron Mountain Ranch Master Plan area. Site Development Plan Review and approval of each development application shall be conducted by the City of Las Vegas Planning Commission at a duly noticed public hearing. Site Development Plan Applications shall be reviewed for consistency with the approved tentative map for the development area and the R-PD Master Plan requirements, standards, and design guidelines with respect to site planning, architecture, and landscape.

Submittal of a Development Plan Review application shall be of a form and content consistent with the City of Las Vegas requirements. If the application is found to be consistent with the R-PD Master Plan and this chapter, the Planning Commission shall approve the application. The decision of the Planning Commission shall be final, subject to appeal to the City Council according to the appeal procedures described below.

8.2.4 Appeal

Appeals from any determination of the Director of Planning and Development shall be made to the Planning Commission. The applicant(s) shall have the right to appeal the decision of the Planning Commission on a site development plan and/or tentative map by filing an application on forms provided by the City of Las Vegas within time period as prescribed by the City of Las Vegas Zoning Code after notice of approval by the Planning Commission. Appeals shall be processed consistent with the applicable provisions of the City of Las Vegas Zoning Code.

8.3 MODIFICATIONS AND AMENDMENTS

8.3.1 Minor Modifications

The following constitute minor modifications to the R-PD Master Plan which are subject to review and approval by the Director of Planning and Development. The Community Development Director shall have the discretion to refer any such request for modification to the Planning Commission.

- Final sizing and precise location of public facilities such as water, sewer, storm drains pursuant to recommendations of the City Engineer.
- Change in utility and/or public service provider.
- Standard residential street and private drive alignments within individual planning area tracts.
- Increase in unit counts of up to 5% for any planning area or village above the Approximate Proposed Units as listed in the Statistical Summary, Section 2.4, provided that the total number of units does not exceed the maximum established in the R-PD Master Plan.
- Transfer of units within one or more planning areas or villages provided the total number of units proposed to be added to a planning area or village does not exceed 5% of the total number of units allowed for the planning area or village as established in the R-PD Master Plan and the total number of units does not exceed that established in the R-PD Master Plan.
- Adjustment of planning area boundaries provided the total acreage of the affected planning area does not increase or decrease by more than 5% of the total stated in the approved R-PD Master Plan.
- Minor changes to landscape materials, wall materials, wall alignment, entry design, and streetscape design which are consistent with the purpose and intent of the design criteria set forth in the R-PD Master Plan.
- Other modifications of a similar nature to those listed above, which are deemed minor by the Community Development Director, which are in keeping with the purpose and intent of the approved R-PD Master Plan and which are in

conformance with the R-PD Zoning District approved for Iron Mountain Ranch.

8.3.2 Significant Modification of the R-PD Master Plan

An application for a significant modification to the R-PD Master Plan may be requested by the applicant at any time pursuant to Chapter 19A.06 (R-PD Planned Development District) of the City of Las Vegas Zoning Code and shall be approved by the City Council upon recommendation of the City Planning Commission.

8.4 PHASED DEVELOPMENT

To ensure that Iron Mountain Ranch is developed in an orderly fashion, development is planned to occur in five phases. The phasing plan may vary due to market conditions, and may be amended subject to approval by City staff. Infrastructure improvements will be constructed in an orderly fashion as development occurs, consistent with the phasing plan. A roadway and utility phasing analysis has been prepared by the project engineer and is subject to the approval of the Traffic Access Analysis Report.

Please refer to the Appendix for the planned phasing of infrastructure including street improvements, water and sewer service. Storm drainage and flood control plans will be developed in accordance with the City of Las Vegas standards in conduction with the preparation of street improvement plans.

8.6 MASTER DEVELOPER DESIGN REVIEW PROCESS

8.6.1 Construction Requiring Review: All lot improvements will require review and approval by the Design Review Committee. Improvements include, but are not limited to:

- Site Plot Plan
- Civil Engineering Site Improvement Plans
- Architectural Plans
- Landscape Plans
- Site Amenities

8.6.2 Plan Requirements: All plans for improvements shall be submitted in accordance with the procedures set forth below. All plans shall be prepared by licensed or otherwise qualified land planners, architects, engineers, or other approved designers.

Selection and use of non-licensed design consultants shall not be allowed unless approved by the Design Review Committee prior to any submittal.

- 8.6.3 Application Response Time:** Upon full receipt of all plans and documents in accordance with the provisions herein, the Committee's approval or disapproval of said plans and documents shall be given to the applicant within a reasonable time, not to exceed 30 business days.
- 8.6.4 Standards of Review:** The Design Review Committee shall review the design of each submission for its compatibility to the overall development and adherence to the R-PD Master Plan and the Declaration. The Design Review Committee will evaluate the layout, circulation, and placement of structures and other site features. Such design review shall include, without limitations, architectural style, building size, building massing, building locations, materials, colors, the relationship of the proposed improvements to existing site features, grading and drainage design, landscape design, special site features, and impacts to surrounding parcels.
- 8.6.5 Plan Submittal Procedures:** One paper sepia of plans no larger than 30" x 42" along with the appropriate completed application shall be delivered to:

**IRON MOUNTAIN RANCH
DESIGN REVIEW COMMITTEE
c/o Champion Homes, Inc.
444 Warm Springs Road, Suite 120
Las Vegas, Nevada 89119**

(Phone) 702 / 896-1988

It is recommended the applicant arrange for a pre-design meeting with the Design Review Committee, or its representative, prior to the submittal or design of the project. A meeting may be scheduled by contacting the Committee at the preceding address or telephone number.

8.6.6 SUBMITTALS

- 8.6.6.a Preliminary Plan Submittal:** Preliminary plans on paper sepia shall be submitted to the Design Review Committee for approval prior to the submission for review of final drawings. All preliminary plans shall be drawn to an appropriate scale. The following information shall be clearly indicated on all drawings submitted:

- Sheet Title
- Scale and North Arrow
- Lot or Parcel Owner
- Prepared By (Entity that prepared the plans)
- Date the drawings were completed and/or modified
- Parcel Identification

The following plans are required for submittal:

1. **Site Plan:** Dimensioned site plan showing parcel and lot boundaries, building footprints, setbacks, easements, paving, accessory structures, existing and proposed grade elevations, and any requested variances.
2. **Architecture Concept Plans:** Dimensioned sketches showing all building elevations, floor plan, floor plan area calculations, and signage.
3. **Landscape Concept Plan:** Sketch showing all proposed landscape elements, location of plant materials, and plant list.

Items 1 and 3 may be combined on one plan if all information is clearly presented.

8.6.6.b Preliminary Plan Approval: Upon review, the Design Review Committee will either approve, approve with conditions, or deny the preliminary plans. The Applicant may submit the final plan only if the preliminary plans are approved or are approved with conditions. The Applicant must address all preliminary plan approval conditions on the final plan. Preliminary plans which are denied must be revised and re-submitted to the Design Review Committee for review.

8.6.6.c Final Plan Review: Final plans on paper sepia are to be submitted for review only after approval or approval with conditions of the preliminary plans has been granted. Final plans shall be drawn to scale. Final plans shall include all of the information requested in the preliminary plan submittal, and additional details and information as outlined below:

The following plans are required for submittal:

1. **Site Plan:** Dimensioned site plan showing lots, setbacks, easements, walls, fences, site amenities, utility and equipment pads, streets, and other paved surfaces.

2. **Plot Plan:** Plot plan showing building footprints, setbacks driveways, walls, and fences for each lot.
3. **Grading Plan:** Grading plans showing all existing and proposed elevations, drainage flow lines, drainage structures, retaining walls, or other site structures.
4. **Improvement Plans:** Plans showing all sanitary sewer, water, storm drain, paving, and street light improvements and utility connections.
5. **Architecture Plans:** Dimensioned floor plans, elevations, roof plans and sign details. Elevations or similar drawings depicting use of exterior materials and paint colors. Construction plans and details are not required and will not be reviewed.
6. **Material Sample Board:** Material sample board showing samples or manufacturer's cut sheets for all exterior materials, including but not limited to the following. Boards shall not exceed 24" x 36".
 - Doors
 - Garage Doors
 - Windows
 - Lighting
 - Roofing
 - Colors
 - Building Wall Finishes
 - Site Wall Finishes
 - Sign Finish
 - Decorative Paving
 - Landscape Gravel
7. **Landscape Plan:** Landscape plan indicating species, size, and location of all plant materials. Landscape plan shall also depict all paving and site amenities.
8. **Construction Method Plan:** Construction method plan showing location of Lot construction access, trash receptacles, construction trailers (if any), temporary utility connections, and temporary restrooms.

Items 1, 3, and 7 above may be combined on one plan if all information is clearly depicted. All plans must be submitted together for review, unless prior permission for partial submittals is granted by the Design Review Committee.

- 8.6.6.d Final Plan Approval:** The Design Review Committee will either approve the plans or request a re-submittal to address any outstanding items.
- 8.6.6.e Building Permits:** No building permit may be received from the City of Las Vegas without prior approval of the final plans by the Design Review Committee.
- 8.6.6.f City of Las Vegas Review:** All projects are subject to the review procedures as set forth in the City of Las Vegas Zoning code. No plans may be submitted to the City of Las Vegas without prior approval of the final plans by the Design Review Committee.

8.7 ADMINISTRATION

- 8.7.1 Fees:** The Design Review Committee shall have the right to require payment of reasonable fees for the review of proposed plans and other materials, and site observation of constructed improvements.

■ Pre-Design Meeting:	No Charge
■ Preliminary Plan Submittal:	\$750
■ Final Plan Submittal:	\$1,000

Reviews in excess of two for each submittal will be charged to the Applicant on an hourly basis at a rate of \$115.00 per hour.

- 8.7.2 Amendment:** The design criteria may be amended by the unanimous vote of the Design Review Committee and approved by the City of Las Vegas.
- 8.7.3 Miscellaneous:** All items submitted to the Design Review Committee shall become the property of the Committee. Changes to the approved plans shall be re-submitted to the Committee for approval and shall clearly identify the revision(s).
- 8.7.4 Prosecution of Work After Approval:** After approval of the final plans by the Design Review Committee, the construction, alteration or other work described therein shall be commenced and completed in accordance with the rules set forth in the R-PD and the Declaration. The Committee or its representative has the right to enter the lot or premises and to inspect the project for compliance with the R-PD or

Declaration at any time, without advance notice to the lot owner nor fear of trespass and liability.

- 8.7.5 Violations:** Construction deemed by the Design Review Committee to be in violation of approved drawings, the R-PD or the Declaration shall be corrected as described in the Declaration.
- 8.7.6 Rule Making Authority:** The Design Review Committee adopts the design criteria for the purpose of interpreting, applying, supplementing and implementing the provisions of the Declaration pertaining to the design of lots, buildings and other improvements. A copy of the R-PD as from time to time adopted, amended or repealed, shall be maintained in the office of the Master Developer and shall be available for inspection during normal business hours by any lot or parcel owner, prospective owner, or any architect or agent of any such owner or prospective owner. It shall be the responsibility of the owner or prospective owner, or architect or agent of any such owner or prospective owner to inform themselves as to any and all such changes of the design criteria.
- 8.7.7 Professional Advice:** The Design Review Committee may employ the services of an architect, attorney, land planner, landscape architect or engineer to render professional advice and may charge the cost for services of such a professional to the applicant, but only after the applicant has been informed in advance such compensation shall be so charged.

CHAPTER 9

APPENDICES

Iron Mountain Ranch

DESIGN GUIDELINES
OCTOBER 12, 1998

**TRAFFIC IMPACT REVIEW
FOR
IRON MOUNTAIN RANCH**

Iron Mountain Ranch

**DESIGN GUIDELINES
OCTOBER 12, 1998**

VTN NEVADA

Contact: David A. Guerra, E.I.

(702) 873-7550

Location: Area bounded by Jones Blvd.,
Decatur Blvd., Iron Mountain Ranch Rd., & Whispering Sands Dr.

W.O. 5407

May 11, 1998

TRAFFIC REPORT

Project Description

The proposed 280± acre site is bounded by Jones Boulevard on the west, Decatur Boulevard on the east, Horse Drive to the north, and Whispering Sands Drive to the south. The project is proposed to consist of approximately 1,325 ± single family homes. Access to the site will be provided by residential collector streets from Jones Boulevard, Decatur Boulevard, Whispering Sands Drive, Bradley Road, and Horse Drive.

Trip Generation

The traffic generated by the project can be estimated based on the data provided in the Institute of Transportation Engineers (ITE) Trip Generation, 6th Edition. This reference is a tool for estimating the number of vehicle trips likely to be generated for a particular land use and is based on many trip generation studies. Vehicle trips are defined as a single or one-direction vehicle movement with either the origin or destination (exiting and entering) inside a study site. The following table summarizes the trip generation expected for this project:

Land Use	Size	24 Hour Volume	AM		PM	
			Enter	Exit	Enter	Exit
Single Family Dwelling (ITE Code 210)	2,427 Units	23,226	455	1365	1569	882

Area Roadways

The project will have frontage along Jones Boulevard, Decatur Boulevard, Whispering Sands Drive, Horse Drive, and Iron Mountain Ranch Road. A portion of Jones Boulevard is an improved asphalt concrete access road. The remaining streets are primarily dirt trails.

Mitigation Measures

As required by the City of Las Vegas, half-street improvements will be constructed on Jones Boulevard, Decatur Boulevard, Whispering Sands Drive, Bradley Road, Iron Mountain Ranch Road, and Horse Drive along the projects perimeter. Full street improvements will be constructed on streets internal of the project site. Access roads will be constructed from the project site to provide access to the surrounding roadway network. These improvements are expected to provide adequate access and circulation for the proposed site. It is expected that future impact studies will be required for this proposed development which will identify the developers participation in further mitigation measures.

VTN NEVADA

Contact: David A. Guerra, E.I.

(702) 873-7550

Location: Area bounded by Jones Blvd.,
Decatur Blvd., Iron Mountain Ranch Rd., & Whispering Sands Dr.

W.O. 5407

May 11, 1998

RIGHT-OF-WAY REPORT

The proposed development is bounded by Jones Boulevard on the west, Decatur Boulevard on the east, Iron Mountain Ranch Road to the north, and Whispering Sands Drive to the south. Right-of-way (ROW) along the north 60' should be dedicated for Iron Mountain Ranch Road adjacent to the site. Right-of-way (ROW) along the east 60' of the should be dedicated Decatur Boulevard adjacent to the site. Right-of-way (ROW) along the west 50' of the site should be dedicated for Jones Boulevard adjacent to the site. Right-of-way (ROW) along the south 30' of the site should be dedicated for Whispering Sands Drive adjacent to the site. Right-of-way (ROW) along other perimeter streets should be dedicated adjacent to the site. These roadways are expected to provide adequate access to the proposed development. No additional ROW should be required.

Iron Mountain Ranch
SUMMARY OF AVERAGE VEHICLE TRIP GENERATION
FOR 2427 DWELLING UNITS OF SINGLE FAMILY DWELLINGS
DRIVEWAY VOLUMES
5/11/98

	24 HOUR TWO-WAY VOLUME	7-9 AM PK HOUR ENTER	EXIT	4-6 PM PK HOUR ENTER	EXIT
AVERAGE WEEKDAY	23226	455	1365	1569	882

	24 HOUR TWO-WAY VOLUME	PEAK HOUR ENTER	EXIT
SATURDAY	24488	1232	1049
SUNDAY	21309	1106	981

Note: A zero rate indicates no rate data available
Source: Institute of Transportation Engineers
Trip Generation, 6th Edition, 1997.

TRIP GENERATION BY MICROTRANS

Center For Microcomputers In Transportation
 University of Florida
 512 Weil Hall
 Gainesville, FL 32611-2083
 Ph: (904) 392-0378

Streets: (N-S) Jones Boulevard (E-W) Whispering Sands
 Analyst..... Peter Sekioka
 Date of Analysis..... 5/8/98
 Other Information..... AM Peak Hour
 All-way Stop-controlled Intersection

	Northbound			Southbound			Eastbound			Westbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	0	1	1	1	1	0	0	0	0	1	0	1
Volumes		161	0	0	484					0		0
PHF		.95	.95	.95	.95					.95		.95

Volume Summary and Capacity Analysis WorkSheet

	NB	SB	EB	WB
LT Flow Rate	0	0		
RT Flow Rate	0	0		
Approach Flow Rate	169	509		
Proportion LT	0.00	0.00		
Proportion RT	0.00	0.00		
Opposing Approach Flow Rate	509	169		
Conflicting Approaches Flow Rate	0	0		
Proportion, Subject Approach Flow Rate	0.25	0.75		
Proportion, Opposing Approach Flow Rate	0.75	0.25		
Lanes on Subject Approach	2	2		
Lanes on Opposing Approach	2	2		
LT, Opposing Approach	0	0		
RT, Opposing Approach	0	0		
LT, Conflicting Approaches	0	0		
RT, Conflicting Approaches	0	0		
Proportion LT, Opposing Approach	0.00	0.00		
Proportion RT, Opposing Approach	0.00	0.00		
Proportion LT, Conflicting Approaches	0.00	0.00		
Proportion RT, Conflicting Approaches	0.00	0.00		
Approach Capacity	975	1125		

Intersection Performance Summary

Movement	Approach Flow Rate	Approach Capacity	V/C Ratio	Average Total Delay	LOS
NB	169	975	0.17	1.9	A
SB	509	1125	0.45	5.6	B

Intersection Delay = 4.7
 Level of Service (Intersection) = A

Center For Microcomputers In Transportation
 University of Florida
 512 Weil Hall
 Gainesville, FL 32611-2083
 Ph: (904) 392-0378

Streets: (N-S) Jones Boulevard (E-W) Whispering Sands
 Analyst..... Peter Sekioka
 Date of Analysis..... 5/8/98
 Other Information..... PM Peak Hour
 All-way Stop-controlled Intersection

	Northbound			Southbound			Eastbound			Westbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	0	1	1	1	1	0	0	0	0	1	0	1
Volumes		557	0	0	313					0		0
PHF		.95	.95	.95	.95					.95		.95

Volume Summary and Capacity Analysis WorkSheet

	NB	SB	EB	WB
LT Flow Rate	0	0		
RT Flow Rate	0	0		
Approach Flow Rate	586	329		
Proportion LT	0.00	0.00		
Proportion RT	0.00	0.00		
Opposing Approach Flow Rate	329	586		
Conflicting Approaches Flow Rate	0	0		
Proportion, Subject Approach Flow Rate	0.64	0.36		
Proportion, Opposing Approach Flow Rate	0.36	0.64		
Lanes on Subject Approach	2	2		
Lanes on Opposing Approach	2	2		
LT, Opposing Approach	0	0		
RT, Opposing Approach	0	0		
LT, Conflicting Approaches	0	0		
RT, Conflicting Approaches	0	0		
Proportion LT, Opposing Approach	0.00	0.00		
Proportion RT, Opposing Approach	0.00	0.00		
Proportion LT, Conflicting Approaches	0.00	0.00		
Proportion RT, Conflicting Approaches	0.00	0.00		
Approach Capacity	1092	1008		

Intersection Performance Summary

Movement	Approach Flow Rate	Approach Capacity	V/C Ratio	Average Total Delay	LOS
NB	586	1092	0.54	7.7	B
SB	329	1008	0.33	3.5	A

Intersection Delay = 6.2

Level of Service (Intersection) = B

Center For Microcomputers In Transportation
 University of Florida
 512 Weil Hall
 Gainesville, FL 32611-2083
 Ph: (904) 392-0378

Streets: (N-S) Decatur Boulevard (E-W) Whispering Sands
 Analyst..... Peter Sekioka
 Date of Analysis..... 5/8/98
 Other Information.....AM Peak Hour
 All-way Stop-controlled Intersection

	Northbound			Southbound			Eastbound			Westbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	1	1	0	0	1	1	1	0	1	0	0	0
Volumes	0	161			484	0	0		0			
PHF	.95	.95			.95	.95	.95		.95			

Volume Summary and Capacity Analysis WorkSheet

	NB	SB	EB	WB
LT Flow Rate	0	0		
RT Flow Rate	0	0		
Approach Flow Rate	169	509		
Proportion LT	0.00	0.00		
Proportion RT	0.00	0.00		
Opposing Approach Flow Rate	509	169		
Conflicting Approaches Flow Rate	0	0		
Proportion, Subject Approach Flow Rate	0.25	0.75		
Proportion, Opposing Approach Flow Rate	0.75	0.25		
Lanes on Subject Approach	2	2		
Lanes on Opposing Approach	2	2		
LT, Opposing Approach	0	0		
RT, Opposing Approach	0	0		
LT, Conflicting Approaches	0	0		
RT, Conflicting Approaches	0	0		
Proportion LT, Opposing Approach	0.00	0.00		
Proportion RT, Opposing Approach	0.00	0.00		
Proportion LT, Conflicting Approaches	0.00	0.00		
Proportion RT, Conflicting Approaches	0.00	0.00		
Approach Capacity	975	1125		

Intersection Performance Summary

Movement	Approach Flow Rate	Approach Capacity	V/C Ratio	Average Total Delay	LOS
NB	169	975	0.17	1.9	A
SB	509	1125	0.45	5.6	B

Intersection Delay = 4.7
 Level of Service (Intersection) = A

Center For Microcomputers In Transportation
 University of Florida
 512 Weil Hall
 Gainesville, FL 32611-2083
 Ph: (904) 392-0378

Streets: (N-S) Decatur Boulevard (E-W) Whispering Sands
 Analyst..... Peter Sekioka
 Date of Analysis..... 5/8/98
 Other Information..... PM Peak Hour
 All-way Stop-controlled Intersection

	Northbound			Southbound			Eastbound			Westbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	1	1	0	0	1	1	1	0	1	0	0	0
Volumes	0	557		313	0		0		0			
PHF	.95	.95		.95	.95		.95		.95			

Volume Summary and Capacity Analysis WorkSheet

	NB	SB	EB	WB
LT Flow Rate	0	0		
RT Flow Rate	0	0		
Approach Flow Rate	586	329		
Proportion LT	0.00	0.00		
Proportion RT	0.00	0.00		
Opposing Approach Flow Rate	329	586		
Conflicting Approaches Flow Rate	0	0		
Proportion, Subject Approach Flow Rate	0.64	0.36		
Proportion, Opposing Approach Flow Rate	0.36	0.64		
Lanes on Subject Approach	2	2		
Lanes on Opposing Approach	2	2		
LT, Opposing Approach	0	0		
RT, Opposing Approach	0	0		
LT, Conflicting Approaches	0	0		
RT, Conflicting Approaches	0	0		
Proportion LT, Opposing Approach	0.00	0.00		
Proportion RT, Opposing Approach	0.00	0.00		
Proportion LT, Conflicting Approaches	0.00	0.00		
Proportion RT, Conflicting Approaches	0.00	0.00		
Approach Capacity	1092	1008		

Intersection Performance Summary

Movement	Approach Flow Rate	Approach Capacity	V/C Ratio	Average Total Delay	LOS
NB	586	1092	0.54	7.7	B
SB	329	1008	0.33	3.5	A

Intersection Delay = 6.2
 Level of Service (Intersection) = B

Iron Mountain Ranch
SUMMARY OF AVERAGE VEHICLE TRIP GENERATION
FOR 2461 DWELLING UNITS OF SINGLE FAMILY DWELLINGS
DRIVEWAY VOLUMES
5/4/98

	24 HOUR TWO-WAY VOLUME	7-9 AM PK HOUR ENTER	EXIT	4-6 PM PK HOUR ENTER	EXIT
AVERAGE WEEKDAY	23552	461	1384	1591	895

	24 HOUR TWO-WAY VOLUME	PEAK HOUR ENTER	EXIT
SATURDAY	24831	1249	1064
SUNDAY	21608	1122	995

Note: A zero rate indicates no rate data available
Source: Institute of Transportation Engineers
Trip Generation, 6th Edition, 1997.

TRIP GENERATION BY MICROTRANS

Iron Mountain Ranch
SUMMARY OF AVERAGE VEHICLE TRIP GENERATION
FOR 297 DWELLING UNITS OF SINGLE FAMILY DWELLINGS
DRIVEWAY VOLUMES
5/4/98

	24 HOUR TWO-WAY VOLUME	7-9 AM PK HOUR ENTER	EXIT	4-6 PM PK HOUR ENTER	EXIT
AVERAGE WEEKDAY	2842	56	167	192	108

	24 HOUR TWO-WAY VOLUME	PEAK HOUR ENTER	EXIT
SATURDAY	2997	151	128
SUNDAY	2608	135	120

Note: A zero rate indicates no rate data available
Source: Institute of Transportation Engineers
Trip Generation, 6th Edition, 1997.

TRIP GENERATION BY MICROTRANS

Iron Mountain Ranch

SUMMARY OF AVERAGE VEHICLE TRIP GENERATION
FOR 209 DWELLING UNITS OF SINGLE FAMILY DWELLINGS
DRIVEWAY VOLUMES
5/4/98

	24 HOUR TWO-WAY VOLUME	7-9 AM PK HOUR ENTER	EXIT	4-6 PM PK HOUR ENTER	EXIT
AVERAGE WEEKDAY	2000	39	118	135	76
	24 HOUR TWO-WAY VOLUME	PEAK HOUR ENTER		EXIT	
SATURDAY	2109	106		90	
SUNDAY	1835	95		84	

Note: A zero rate indicates no rate data available

Source: Institute of Transportation Engineers

Trip Generation, 6th Edition, 1997.

TRIP GENERATION BY MICROTRANS

Iron Mountain Ranch
SUMMARY OF AVERAGE VEHICLE TRIP GENERATION
FOR 417 DWELLING UNITS OF SINGLE FAMILY DWELLINGS
DRIVEWAY VOLUMES
5/4/98

	24 HOUR TWO-WAY VOLUME	7-9 AM PK HOUR ENTER	EXIT	4-6 PM PK HOUR ENTER	EXIT
AVERAGE WEEKDAY	3991	78	235	270	152
	24 HOUR TWO-WAY VOLUME	PEAK HOUR		ENTER	EXIT
SATURDAY	4208			212	180
SUNDAY	3661			190	169

Note: A zero rate indicates no rate data available
Source: Institute of Transportation Engineers
Trip Generation, 6th Edition, 1997.

TRIP GENERATION BY MICROTRANS

Iron Mountain Ranch
SUMMARY OF AVERAGE VEHICLE TRIP GENERATION
FOR 344 DWELLING UNITS OF SINGLE FAMILY DWELLINGS
DRIVEWAY VOLUMES
5/4/98

	24 HOUR TWO-WAY VOLUME	7-9 AM PK HOUR ENTER	EXIT	4-6 PM PK HOUR ENTER	EXIT
AVERAGE WEEKDAY	3292	65	194	222	125
	24 HOUR TWO-WAY VOLUME	PEAK HOUR		ENTER	EXIT
SATURDAY	3471	175		149	
SUNDAY	3020	157		139	

Note: A zero rate indicates no rate data available

Source: Institute of Transportation Engineers

Trip Generation, 6th Edition, 1997.

TRIP GENERATION BY MICROTRANS

Iron Mountain Ranch
SUMMARY OF AVERAGE VEHICLE TRIP GENERATION
FOR 342 DWELLING UNITS OF SINGLE FAMILY DWELLINGS
DRIVEWAY VOLUMES
5/4/98

	24 HOUR TWO-WAY VOLUME	7-9 AM PK HOUR ENTER	EXIT	4-6 PM PK HOUR ENTER	EXIT
AVERAGE WEEKDAY	3273	64	192	221	124

	24 HOUR TWO-WAY VOLUME	PEAK HOUR ENTER	EXIT
SATURDAY	3451	174	148
SUNDAY	3003	156	138

Note: A zero rate indicates no rate data available
Source: Institute of Transportation Engineers
Trip Generation, 6th Edition, 1997.

TRIP GENERATION BY MICROTRANS

Iron Mountain Ranch
SUMMARY OF AVERAGE VEHICLE TRIP GENERATION
FOR 702 DWELLING UNITS OF SINGLE FAMILY DWELLINGS
DRIVEWAY VOLUMES
5/4/98

	24 HOUR TWO-WAY VOLUME	7-9 AM PK HOUR ENTER	EXIT	4-6 PM PK HOUR ENTER	EXIT
AVERAGE WEEKDAY	6718	132	395	454	255

	24 HOUR TWO-WAY VOLUME	PEAK HOUR ENTER	EXIT
SATURDAY	7083	356	304
SUNDAY	6164	320	284

Note: A zero rate indicates no rate data available

Source: Institute of Transportation Engineers

Trip Generation, 6th Edition, 1997.

TRIP GENERATION BY MICROTRANS

Iron Mountain Ranch

SUMMARY OF AVERAGE VEHICLE TRIP GENERATION
FOR 150 DWELLING UNITS OF SINGLE FAMILY DWELLINGS
DRIVEWAY VOLUMES
5/4/98

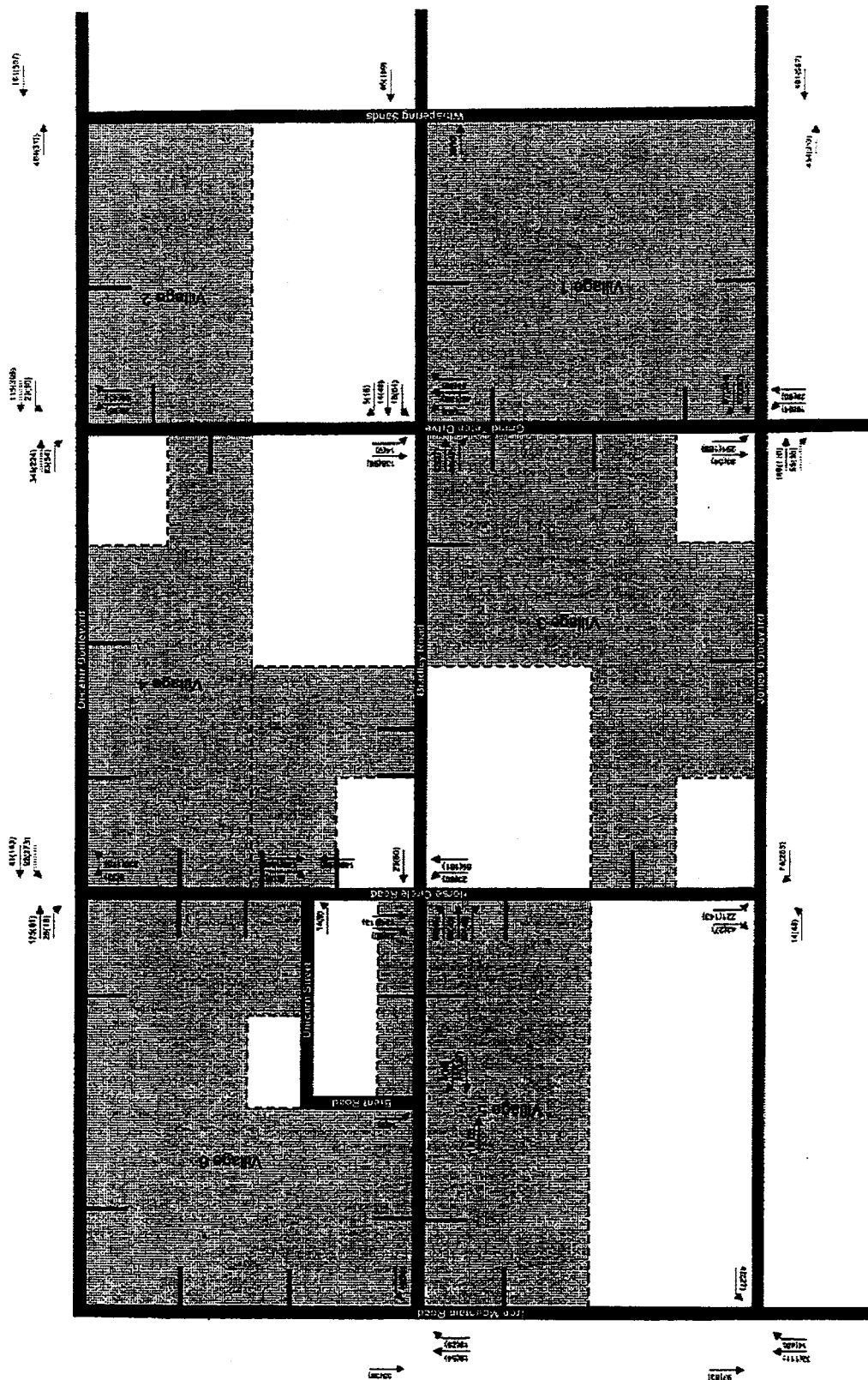
	24 HOUR TWO-WAY VOLUME	7-9 AM PK HOUR ENTER	EXIT	4-6 PM PK HOUR ENTER	EXIT
AVERAGE WEEKDAY	1436	28	84	97	55

	24 HOUR TWO-WAY VOLUME	PEAK HOUR ENTER	EXIT
SATURDAY	1514	76	65
SUNDAY	1317	68	61

Note: A zero rate indicates no rate data available

Source: Institute of Transportation Engineers
Trip Generation, 6th Edition, 1997.

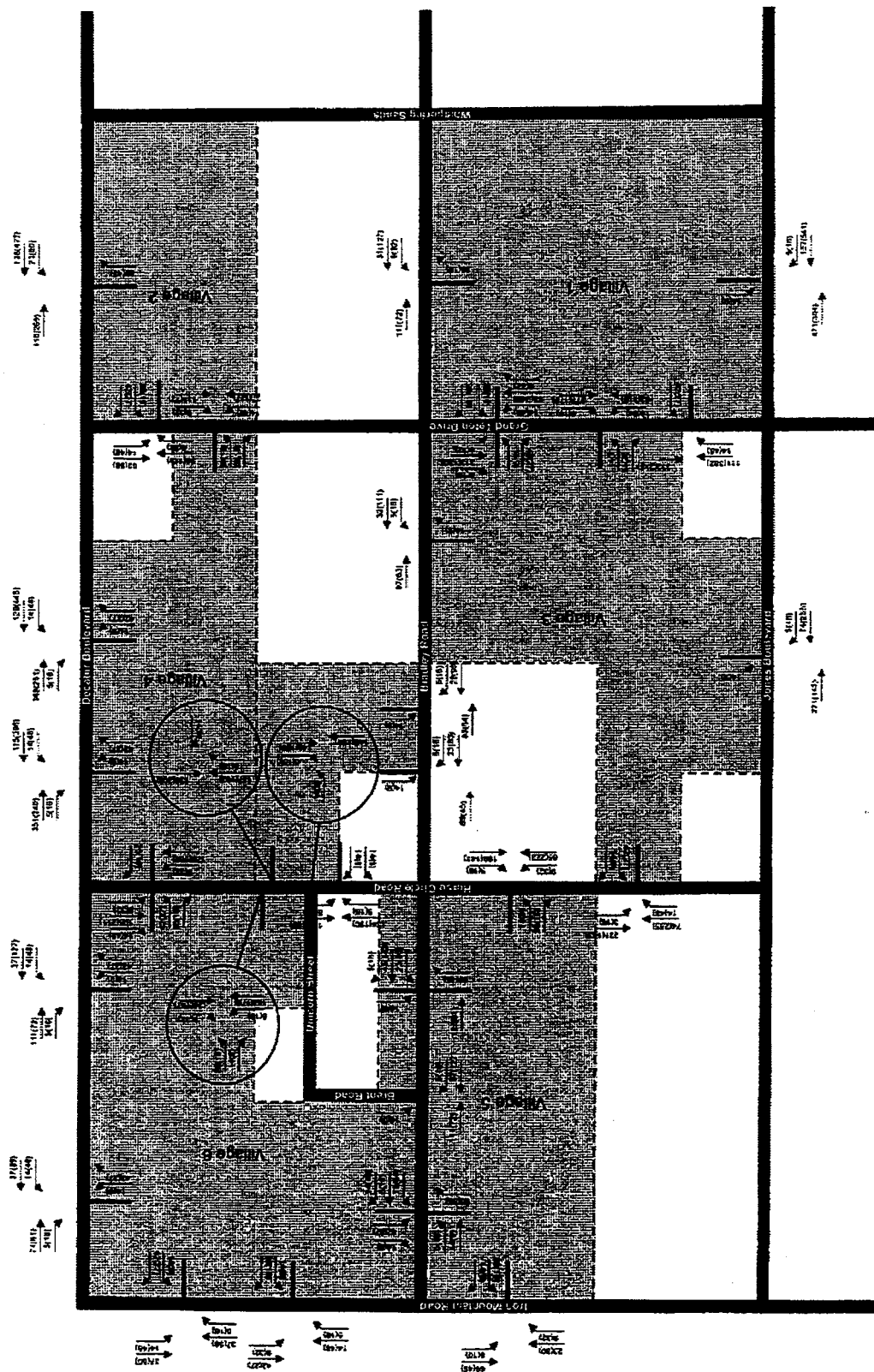
TRIP GENERATION BY MICROTRANS



PEAK HOUR TRAFFIC VOLUMES
EXISTING BACKGROUND w/ SITE

LEGEND
AM(PM)

**INTERSECTION VOLUMES
IRON MOUNTAIN**



**PEAK HOUR TRAFFIC VOLUMES
EXISTING BACKGROUND w/ SITE**

LEGEND
AM(PM)

**DRIVEWAY VOLUMES
IRON MOUNTAIN**

FIGURE 2
vtn^{cevad}

**IRON MOUNTAIN RANCH
ROADWAY AND UTILITY CONSTRUCTION PHASING
ANALYSIS**

Iron Mountain Ranch

**DESIGN GUIDELINES
OCTOBER 12, 1998**

IRON MOUNTAIN RANCH
ROADWAY AND UTILITY CONSTRUCTION PHASING
ANALYSIS

PURPOSE:

The following report gives preliminary opinions for the proposed utility and street improvements that will be required to construct each of the five (5) phases of development shown on the "Conceptual Master Plan Iron Mountain Ranch". The report focuses only on the infrastructure street, infrastructure utility, and off-site utility improvements. Assumptions are made based on available information.

INFRASTRUCTURE STREETS:

For purposes of this report, only street improvements in the $\frac{1}{4}$, $\frac{1}{2}$ and full section line alignment will be analyzed. The north-south direction the streets are Jones Boulevard, Dunneville Street, Bradley Road, Thom Boulevard and Decatur. The east-west direction the streets are Whispering Sands Drive, Grand Teton Drive, Racel Street, Horse Drive, Brett Lane and Iron Mountain Road.

UTILITY IMPROVEMENTS:

With the exception of off-site improvements, only sewer and water improvements located in the aforementioned infrastructure streets will be examined.

PHASE 1:

Phase 1 is generally bounded by Jones Boulevard to the West, Decatur Boulevard to the East, Grand Teton Drive to the North, and Whispering Sands Drive to the South.

Street improvements surrounding the park site will be a minimum width required to eliminate a saw tooth condition for the construction of Phase One. This will be addressed separately through agreement between the developer and the City of Las Vegas based on park frontage construction.

INFRASTRUCTURE STREET IMPROVEMENTS:

Jones Boulevard (80' R.O.W.)- full half street improvements will be required between Grand Teton Drive and Whispering Sands Drive, a distance of approximately 1350 LF. Depending on progress of projects pending to the south of Phase 1, improvements to Jones Boulevard may be required south of Whispering Sands to provide proper access to the site. The approved 270 acre development project directly to the south has been

conditioned to widen Jones Boulevard to four (4) lanes from Ann Road to Severance Lane.

Grand Teton Drive (120' R.O.W.)- full half street improvements will be constructed between Jones Boulevard and Bradley Road, and between Thom Boulevard and Decatur Boulevard. This will approximately equal 3850 LF of half street improvements. A minimum of 1350 LF of 24 foot temporary access road will be required between Bradley Road and Thom Boulevard.

Decatur Boulevard (120' R.O.W.)- full half street improvements will be required for approximately 1350 LF between Whispering Sands Drive and Grand Teton Drive. The development to the south has been conditioned to construct 2 lanes of access paving streets.

Bradley Road (60' R.O.W.)- full half street improvements will be required for approximately 1350 LF (LF) between Whispering Sands Drive and Grand Teton Drive.

Whispering Sands Drive (60' R.O.W.)- full half street improvements will be required for approximately 3850 LF between Jones Boulevard and Bradley Road and between Thom Boulevard and Decatur Boulevard. Approximately 1350 LF of 24 foot temporary access road will be required between Bradley Road and Thom Boulevard.

OFF-SITE UTILITY IMPROVEMENTS:

Water- The project lies entirely within the 2538 Pressure Zone. The Las Vegas Valley Water District connection for the site is at the intersection of Torrey Pines and Centennial Parkway. The Water District will require that 30 inch oversize water main be brought to the site along the Torrey Pines alignment for the approximately 4,000 LF beginning at the point of connection at Azure Street to the intersection of Torrey Pines and Deer Springs Way. The proposed connection into the 24 inch water main in Farm Road is dependent upon the development to the south constructing a 24 inch water main from Buffalo Road to Jones Boulevard. An oversize agreement must be completed and approved by the Water District prior to submittal of any plans to the Water District for review and subsequent approval. Terms of the agreement will be based on a hydraulic analysis used to determine the developer required diameter of the pipeline.

Grand Teton Drive- approximately 5260 LF of 12 inch water main between Decatur Boulevard and Jones Boulevard.

Jones Boulevard - approximately 3300 LF of 36 inch oversize water main along Iron Mountain Ranch frontage. The water main will be increased to a 36 inch oversize in the Deer Springs Way

alignment for approximately 2600 LF to Jones Boulevard. In Jones Boulevard the 36 inch water main will continue north for approximately 9500 LF.

Sewer- The City of Las Vegas will require the extension of the 15 inch Bradley Road sewer from a point of connection at Bullring Lane to the site as a condition of approval of the zoning. The length of this extension is approximately 10,560 LF.

The development directly south of Phase 1 will extend sewer up Bradley Road to Whispering Sands Drive.

Bradley Road- approximately 10,560 LF of 15 inch between Whispering Sands Drive and Bullring Lane.

INFRASTRUCTURE UTILITY IMPROVEMENTS:

Anticipated main line diameters shown are based on the submitted Hydraulic Analysis to the Las Vegas Valley Water District and preliminary sewer contributions.

Water-

Jones Boulevard- approximately 1350 LF of 36 inch between Grand Teton and Whispering Sands Drive.

Grand Teton Drive- approximately 5280 LF of 12 inch between Jones Boulevard and Decatur Boulevard.

Decatur Boulevard- approximately 1350 LF of 12 inch between Grand Teton and Whispering Sands Drive.

Bradley Road- 1350 LF of 10 inch between Grand Teton and Whispering Sands Drive.

Whispering Sands Drive- approximately 5280 LF of 12 inch between Jones Boulevard and Decatur Boulevard.

Sewer-

Jones Boulevard- approximately 1350 LF of 8 inch between Whispering Sands Drive and Grand Teton Drive.

Bradley Road- approximately 1350 LF of 10 inch between Whispering Sands Drive and Grand Teton Drive.

Whispering Sands Drive- approximately 2460 LF of 8 inch between Jones Boulevard and Bradley Road, approximately 1260 LF of 8 inch between the Thom Boulevard alignment and Decatur Boulevard.

PHASE 2:

Phase 2 is generally bounded by Jones Boulevard to the west, Decatur Boulevard to the east, Racel Street to the north, and Grand Teton Drive to the south.

INFRASTRUCTURE STREET IMPROVEMENTS:

Jones Boulevard (80' R.O. W.)- full half street improvements for approximately 700 LF from the Racel Street intersection south; 24 foot access paving street pavement for approximately 700 if from the Grand Teton Drive intersection north.

Grand Teton Drive (120' R.O.W.)- full half street improvements beginning 660 foot east of Jones Boulevard to Bradley Road. Full half street improvements between Thom Boulevard and Moffat Circle. Remaining portions will be widened access paving street.

Bradley Road (60' R.O.W.)- full half street improvements for approximately 1350 LF between Grand Teton and Racel and widen pavement section on the east side of Bradley.

Decatur Boulevard (120' R.O.W.)- full half street improvements for approximately 700 LF between Grand Teton and Racel and widen pavement section fronting the not a part portion of Iron Mountain Ranch.

Thom Boulevard (51' R.O.W.)- full half street improvements for approximately 1350 LF between Grand Teton and Racel and one travel lane west of centerline.

Racel Street – One travel lane with Phase 3 between Bradley and Thom.

INFRASTRUCTURE UTILITY IMPROVEMENTS:

Water-

Jones Boulevard- approximately 1350 LF of 36 inch between Grand Teton Drive and Racel Street.

Decatur Boulevard- approximately 1350 LF of 12 inch between Grand Teton Drive and Racel Street.

Bradley Road- approximately 1350 LF of 10 inch between Grand Teton Drive and Racel Street.

Thom Boulevard- approximately 1350 LF of 8 inch between Grand Teton and Racel Street.

Sewer-

Bradley Road- approximately 1350 LF of 10 inch between Grand Teton Drive and Racel Street.

Thom Boulevard- approximately 1350 LF of 8 inch between Grand Teton Drive and Racel Street.

PHASE 3:

Phase 3 is generally bounded by Jones Boulevard on the west, Decatur Boulevard on the east, Horse Drive on the north, and the Racel Street alignment to the south.

INFRASTRUCTURE STREET IMPROVEMENTS:

Jones Boulevard (80' R.O.W.)- full half street improvements for approximately 700 LF north of Racel Street, widen access paving street for approximately 700 LF south of Horse Drive.

Decatur Boulevard (120' R.O.W.)- full half street improvements for approximately 1300 LF between Horse Drive and Racel Street.

Horse Drive (80' R.O.W.)- widen access paving street for approximately 650 LF east of Jones Boulevard, then full half street improvements for approximately the next 650 LF east; full half street improvements for approximately 1950 LF between west of Thom Boulevard and Decatur Boulevard.

Racel Street (51' R.O.W.)- Full half street improvements between Bradley and Thom.

INFRASTRUCTURE UTILITY IMPROVEMENTS:

Water-

Jones Boulevard- approximately 700 LF of 36 inch between Racel Street and Horse Drive.

Horse Drive- approximately 4000 LF of 10 inch between Dunneville Street and Decatur Boulevard.

Decatur Boulevard- approximately 1300 LF of 12 inch between Horse Drive and Racel Street.

Thom Boulevard- approximately 1300 LF of 8 inch between Horse Drive and Racel Street.

Sewer-

Bradley Road- approximately 1300 LF of 10 inch between Horse Drive and Racel Street.

Thom Boulevard- approximately 1300 LF of 8 inch between Horse Drive and Racel Street.

PHASE 4:

Phase 4 is generally located between Horse Drive and the Brent Lane alignment from Decatur Boulevard west to approximately 1300 LF beyond Bradley Road. Improvements on the existing Unicorn Street may be required in addition to those listed below.

INFRASTRUCTURE STREET IMPROVEMENTS:

Horse Drive (80' R.O.W.)- approximately 3600 LF of full half street improvements from Decatur Boulevard west, 330 LF of widen access paving street is required west of Unicorn Street. Transitional temporary pavement of approximately 250 LF will be required to tie into pavement constructed with Phase 3 improvements on the south side of Horse Drive and 1320 LF widened pavement to Jones Boulevard.

Decatur Boulevard (120' R.O.W.)- full half street improvements for approximately 1300 LF between Horse Drive and the Brent Lane alignment.

Bradley Road (60' R.O.W.)- full width street improvements for approximately 1300 LF between Brent Lane and Horse Drive.

Brent Lane (60' R.O.W.)- full width street improvements for approximately 650 LF east of the Brent Lane Bradley Road intersection.

Unicorn Street (51' R.O.W.)- Full half street improvements with widened pavement section west of centerline.

INFRASTRUCTURE UTILITY IMPROVEMENTS:

Water-

Horse Drive- constructed with Phase 3.

Decatur Boulevard- approximately 1300 LF of 12 inch between Horse Drive and the Brent Lane alignment.

Bradley Road- approximately 1300 LF of 10 inch between Horse Drive and Brent Lane.

Unicorn Street- approximately 950 LF of 8 inch north of Horse Drive.

Sewer-

Horse Drive- approximately 1300 LF of 8 inch from Bradley Road west; approximately 1300 LF of 8 inch between Bradley Road and Thom Boulevard; approximately 1300 LF of 10 inch between Thom Boulevard and Decatur Boulevard.

Bradley Road- approximately 2600 LF of 8 inch sewer between Brent Lane and the Racel Street alignment.

Unicorn Street- approximately 950 LF of 8 inch north of Horse Drive.

PHASE 5:

Phase 5 is generally located between the Brent Lane alignment and Iron Mountain Road, from Decatur Boulevard west to approximately 1300 LF beyond Bradley Road.

INFRASTRUCTURE STREET IMPROVEMENTS:

Decatur Boulevard (120' R.O.W.)- full half street improvements for approximately 1300 LF between the Brent Lane alignment and Iron Mountain Road.

Iron Mountain Road (60' R.O.W.)- full half street improvements for approximately 3980 LF from Decatur Boulevard west.

Bradley Road (80' R.O.W.)- full width street improvements for approximately 1350 LF between Brent Lane and Iron Mountain Road.

INFRASTRUCTURE UTILITY IMPROVEMENTS:

Water-

Decatur Boulevard- approximately 1300 LF of 12 inch between the Brent Lane alignment and Iron Mountain Road.

Iron Mountain Road- approximately 3980 LF of 12 inch from Decatur Boulevard west.

Bradley Road- approximately 1350 LF of 10 inch from Brent Lane to Iron Mountain Road.

Sewer-

Bradley Road- approximately 1350 LF of 8 inch between Brent Lane and Iron Mountain Road.

Iron Mountain Road- sewer in this road would not be utilized for this development or development to the south. Sewer stubs for future development to the south will be located at the following Iron Mountain Road intersections: Jones Blvd., Bradley Rd., and Decatur Blvd.

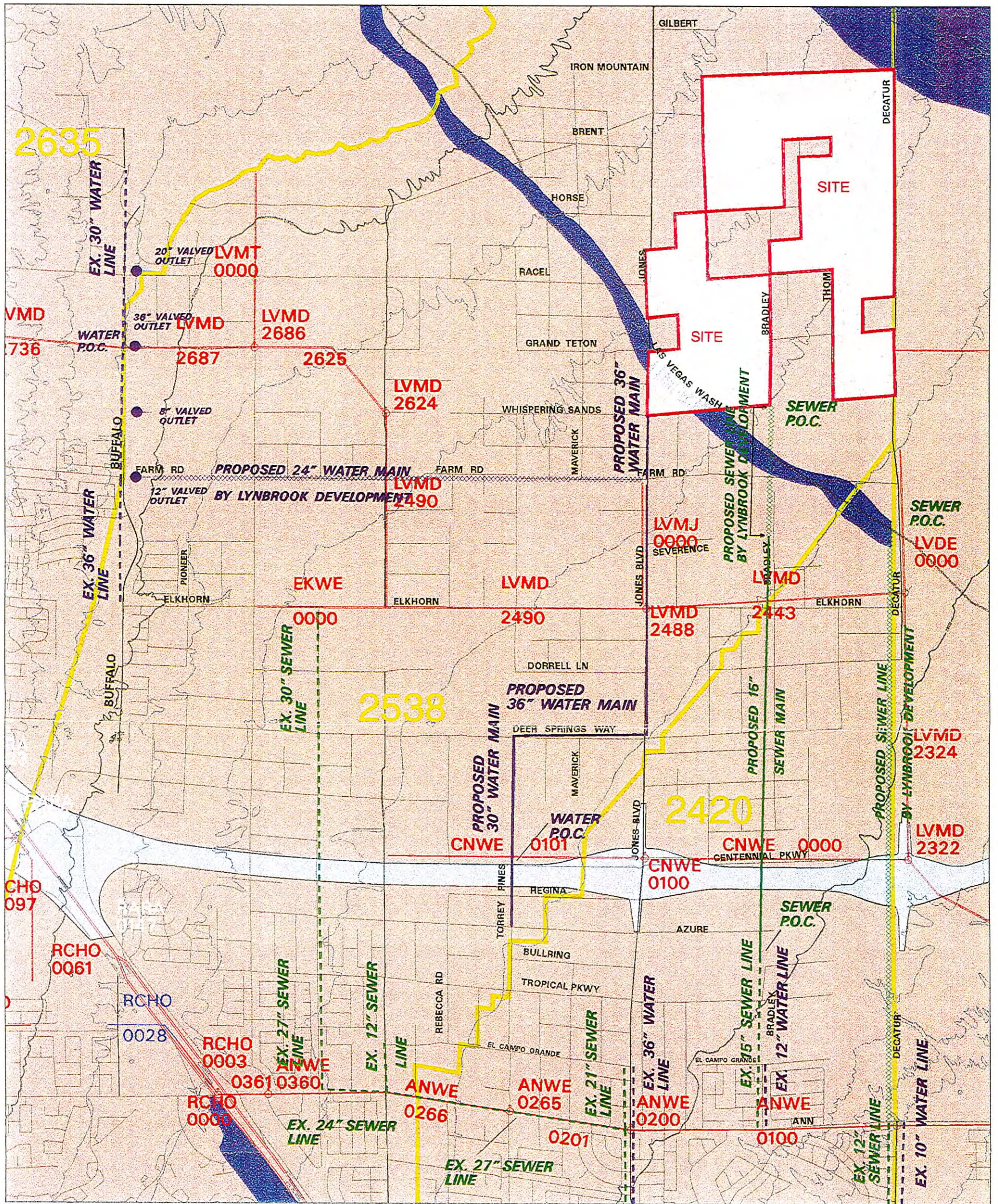
NOTES:

- 1) All lengths and quantities of improvements are estimates and, except for the off-site improvements, were generally obtained from the site plan "Conceptual Master Plan Iron Mountain Ranch".
- 2) All lengths, quantities, and sizes of improvements may lessen or increase pending approval of the traffic impact analysis, overall hydraulic analysis, and completion of an overall sewer system study.
- 3) All street right-of-way reductions are predicated on the approval of the Traffic Access Analysis report and the amendment of the street and highway.

- 3) The City of Las Vegas and Las Vegas Valley Water District may require additional improvements, oversizing of improvements, and improvements to be constructed prior to the phase in which they are shown to be constructed in this report.
- 4) The City of Las Vegas typically requires overpave on all perimeter half street improvements of 14 feet past the centerline of the street.
- 5) This report is an opinion only as to when the improvements described herein will be constructed.

CONCLUSIONS:

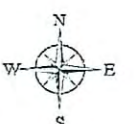
Should the improvements be constructed as phased in this report, it is the engineers opinion that each phase of the project should have adequate traffic circulation, water, and sewer to properly serve the site.



Scale: 1 inch = 900 feet

IRON MOUNTAIN RANCH

- | | | |
|---|----------------|----------------------------|
| WATER LINES | Pressure Zones | Zone X (Outside 500 year) |
| Proposed Water Line | SITE BOUNDARY | Zone A (100 year) |
| Proposed Water Line by Lynbrook development | Valved Outlets | |
| Existing Water Line | Beltway | Proposed FCF |
| SEWER LINES | | Completed FCF |
| Proposed Sewer Line | | |
| Proposed Sewer Line by Lynbrook Development | | |
| Existing Sewer Line | | |



This map is for display purposes only.
No representation is made concerning the
accuracy of the data delineated herein.

Date: June 30, 1998
Mmp: /usr3/projects/landdev/5407/ironmtl698.mmp
Revised: July 21, 1998